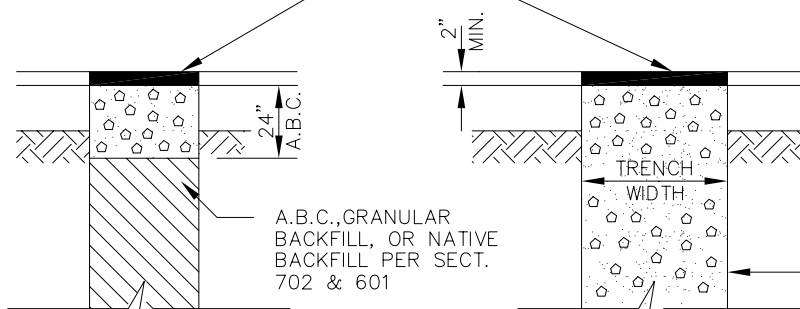


A.C. PAVEMENT MATCH GRADATION & THICKNESS
OF EXISTING PAVEMENT AND COURSES

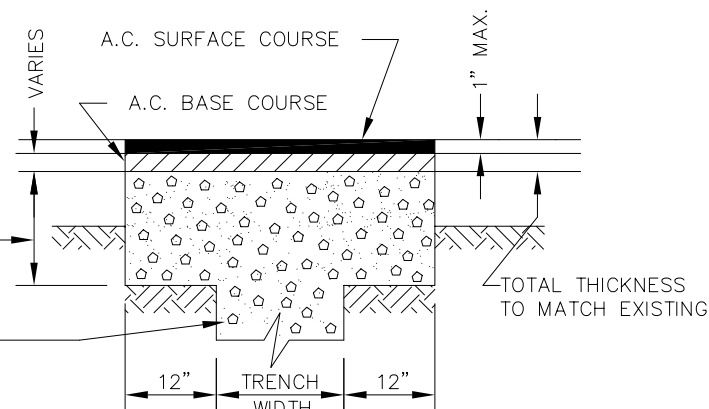


TYPE A

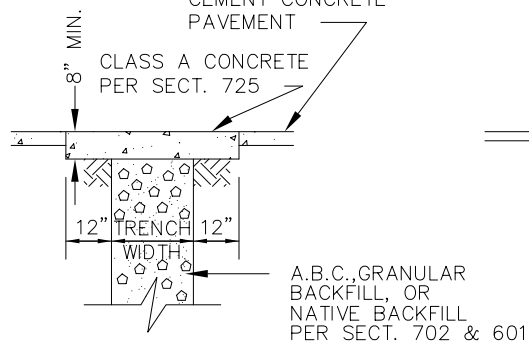
TYPE B

12" A.B.C. OR EXISTING SUBGRADE WHICHEVER IS GREATER

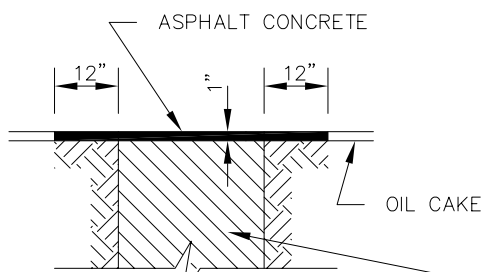
A.B.C. PER SECT. 702 & 601



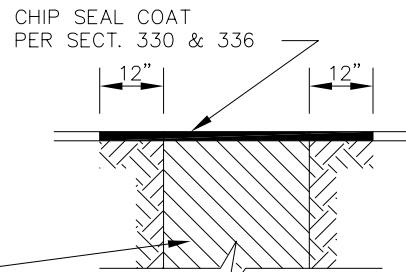
"T" TOP (SEE NOTE 3)



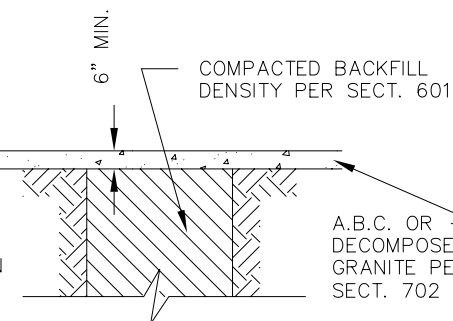
TYPE C



TYPE D



TYPE E



TYPE F

NOTES

1. BEDDING PER SECTION 601.
2. ASPHALT CONCRETE SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF SECT. 321.
3. 12" LIP IS REQUIRED ON THE SIDES OF A TRENCH THAT ARE NOT PARALLEL TO THE CENTER LINE OF THE STREET.
4. TYPES D & E REQUIRE 9" OF A.B.C. AT TOP OF TRENCH WHEN THERE IS AN EXISTING BASE.

SURFACE OUTSIDE OF TRENCH LINES DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO ORIGINAL THICKNESS AND CONDITION.

DETAIL NO.
G-3200

CITY OF GOODYEAR
STANDARD DETAIL

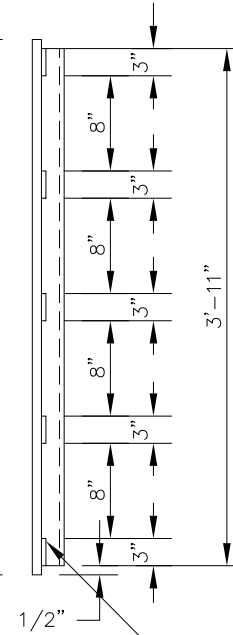
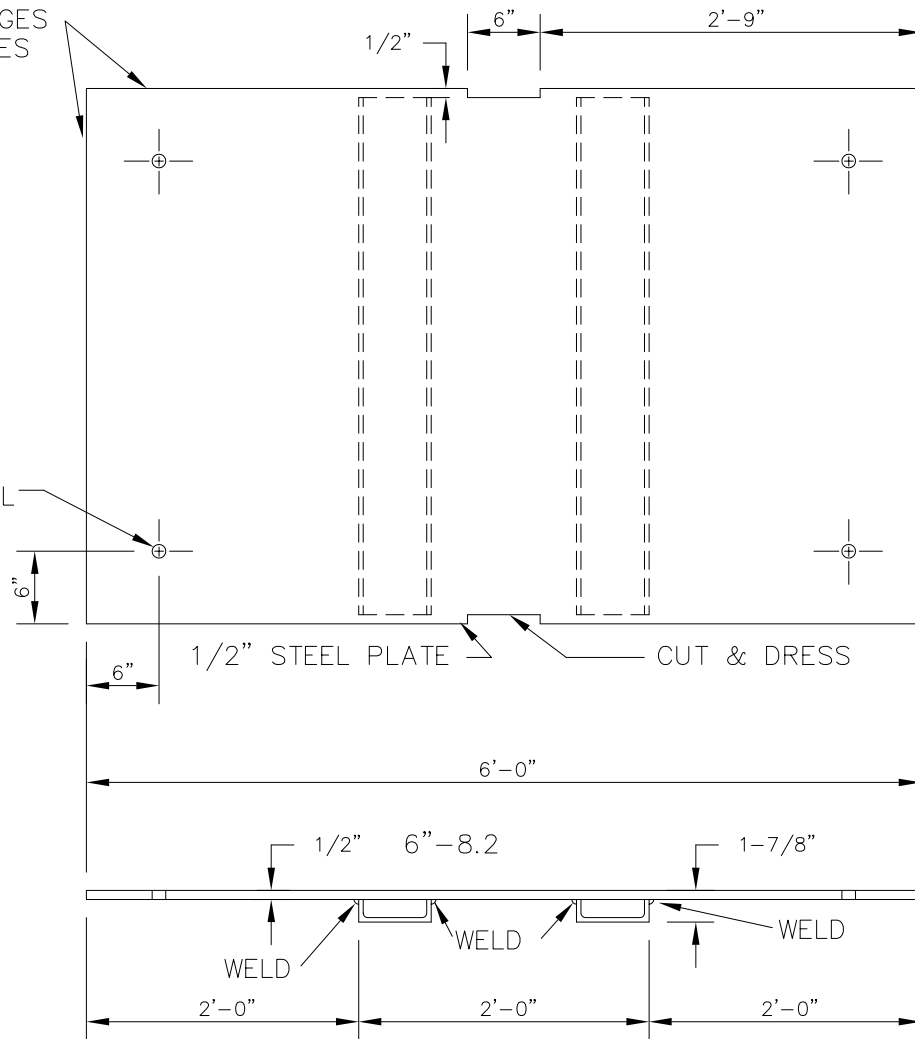
APPROVED BY:
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Policies Committee 7/97

BACKFILL, PAVEMENT & SURFACE REPLACEMENT

DETAIL NO.
G-3200

SMOOTH EDGES
ON ALL SIDES

4 HOLES—
3/4" DRILL



3/8" FILLET WELDS
APPROXIMATELY AS SHOWN,
BOTH SIDES OF CHANNELS.

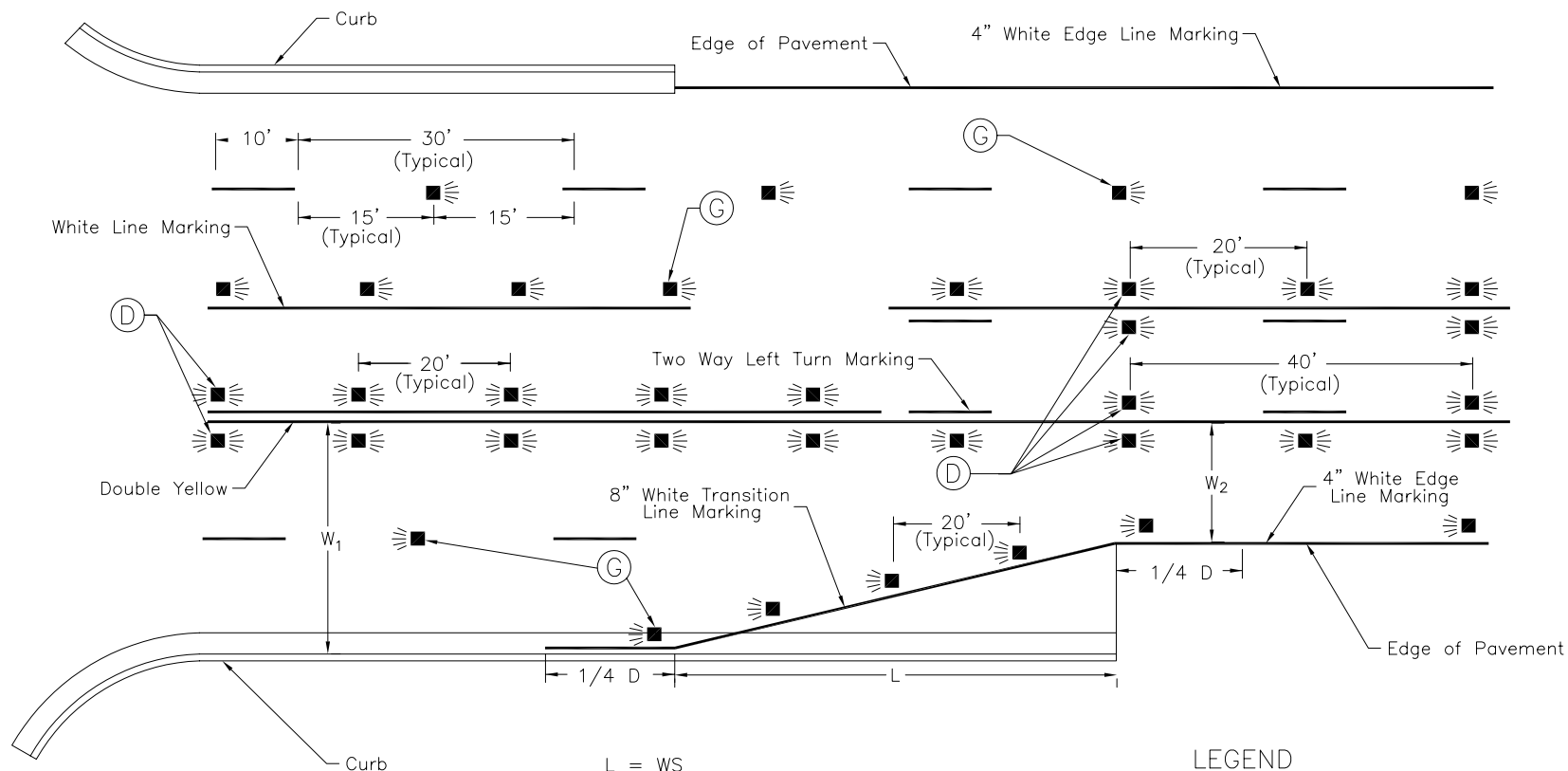
DETAIL NO.
G-3202

CITY OF GOODYEAR
STANDARD DETAIL

APPROVED BY:
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Policies Committee
7/97

STEEL COVER FOR OPEN TRENCHES

DETAIL NO.
G-3202



$L = WS$

$D = 15S$

$W = W_1 - W_2$

$S = \text{DESIGN SPEED OR WHICH EVER IS HIGHER}$

LEGEND

- (G) - TYPE "G" WHITE, ONE WAY REFLECTIVE
 (D) - TYPE "D" YELLOW, TWO WAY REFLECTIVE

SEE STANDARD DETAIL G-3212
FOR FIRE HYDRANT MARKERS

DETAIL NO.
G-3210

CITY OF GOODYEAR
STANDARD DETAIL

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Policies Committee 7/97

PAVEMENT MARKER LAYOUT

DETAIL NO.
G-3210

Figure 1
TWO LANE STREET

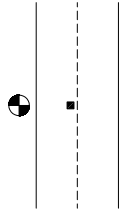


Figure 2
MULTI-LANE STREET

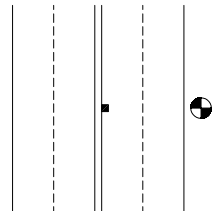


Figure 3
TWO LANE STREET
AT INTERSECTION

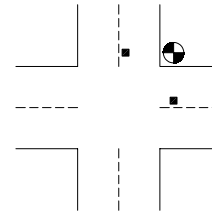


Figure 4
FOUR LANE STREET WITH
TURN LANE AT INTERSECTION

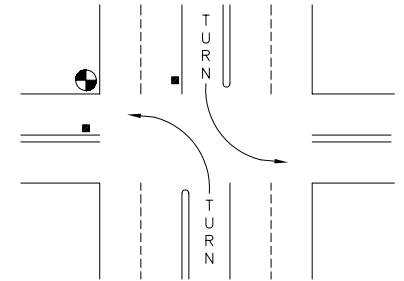


Figure 5
MULTI-LANE STREET WITH
TURN LANE

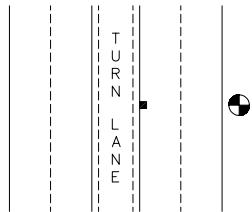


Figure 6
MULTI-LANE STREET
WITH MEDIAN

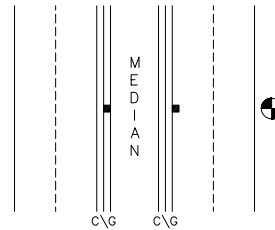
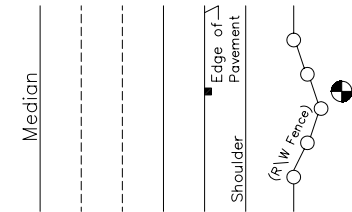


Figure 7
MULTI-LANE STREET
WITH MEDIAN



PAVEMENT MARKERS SHALL BE 911-A
BLUE, FIRE LITE, AMERACE CORPORATION,
SIGNAL PRODUCTS DIVISION OR EQUAL.

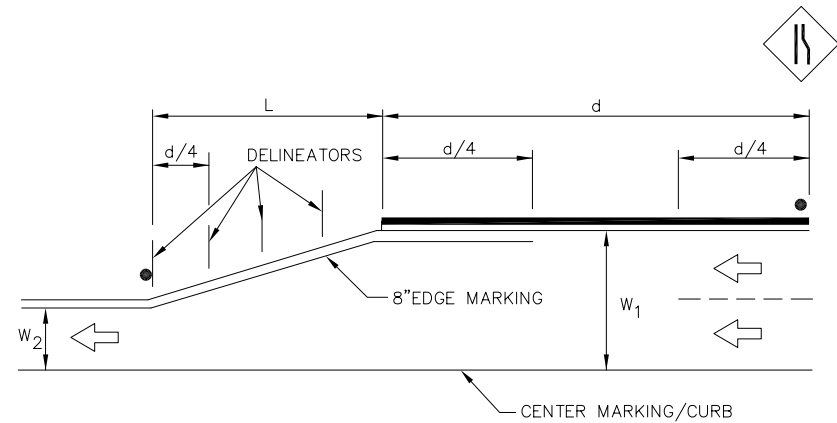
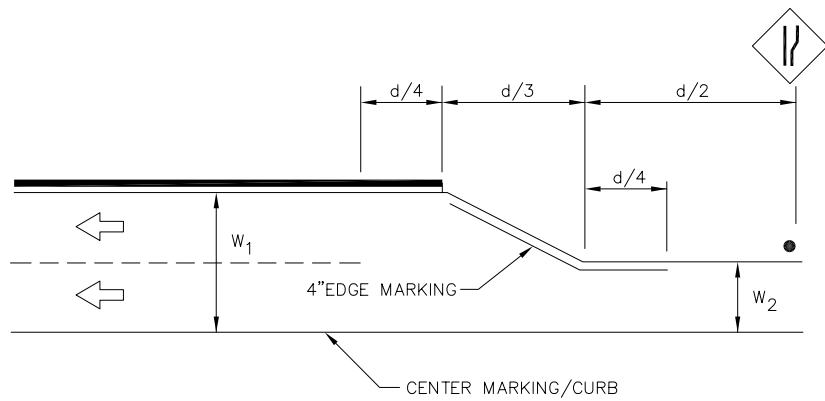
DETAIL NO.
G-3212

CITY OF GOODYEAR
STANDARD DETAIL

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TYPICAL HYDRANT MARKER LOCATION

DETAIL NO.
G-3212



S = DESIGN SPEED OR 85 PERCENTILE SPEED, WHICHEVER IS HIGHER

$$W = W_1 - W_2$$

$$L = WS$$

$$d = 15S$$

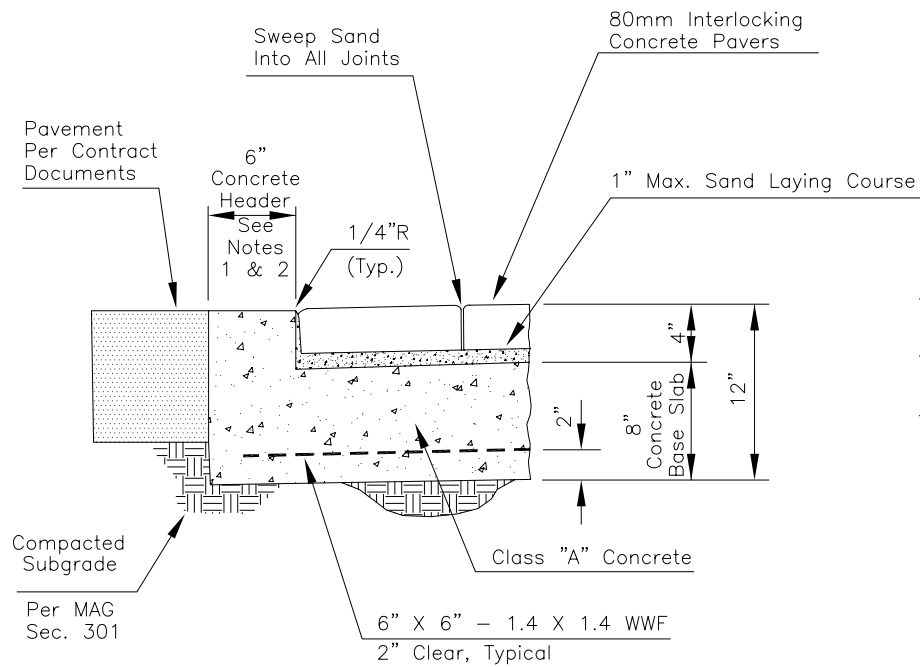
DETAIL NO.
G-3214

CITY OF GOODYEAR
STANDARD DETAIL

APPROVED BY:
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Policies Committee
7/97

PAVEMENT WIDTH TRANSITIONS

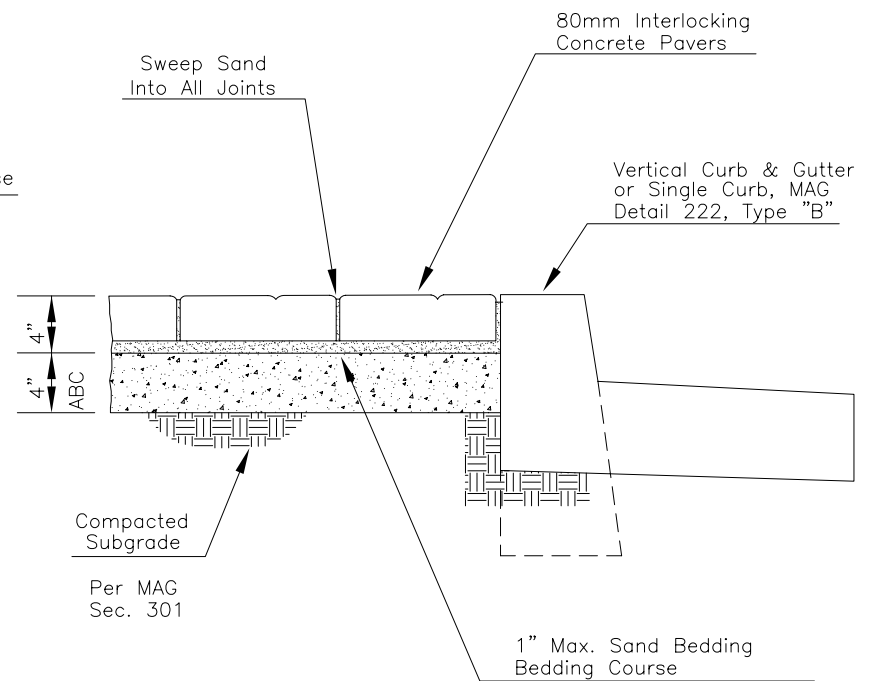
DETAIL NO.
G-3214



Type "B"
Flush Median

NOTES:

1. 1/2" Expansion Joint, ASTM D-1751
Per MAG Sec. 729, Every 50'.
2. Contraction Joints, Every 10'
3. Materials And Construction
MAG Specifications Sec. 342.



Type "A"
Flush Median

DETAIL NO.
G-3220

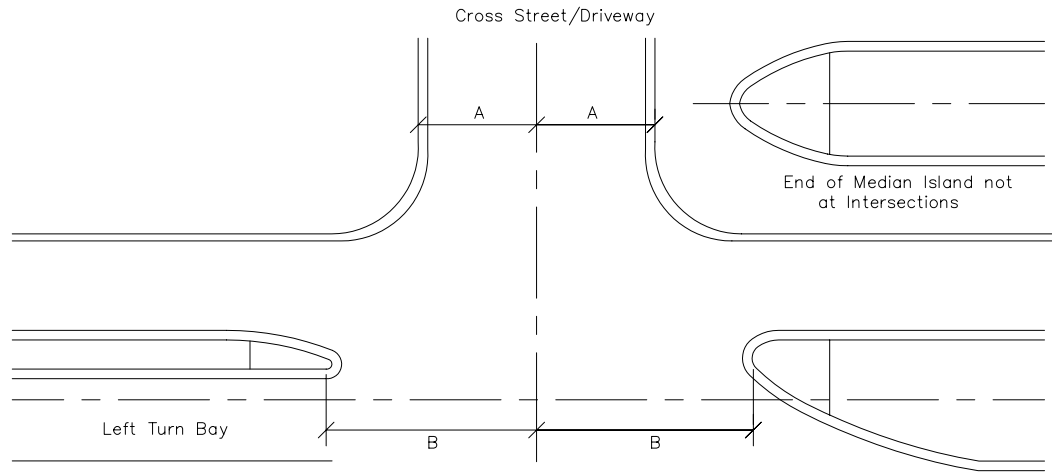
CITY OF GOODYEAR
STANDARD DETAIL

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Goodyear Standards and
Policies Committee

7/97

MEDIAN CONCRETE PAVERS

DETAIL NO.
G-3220



$$B = A + 14; 40' \text{ minimum}$$

NOTES:

1. This sketch is for a three leg intersection. If the intersection has four legs, the right side will also have an auxiliary lane for left turns, and the median on the right side will have the same configuration as the one on the left side rotated 180 degrees.

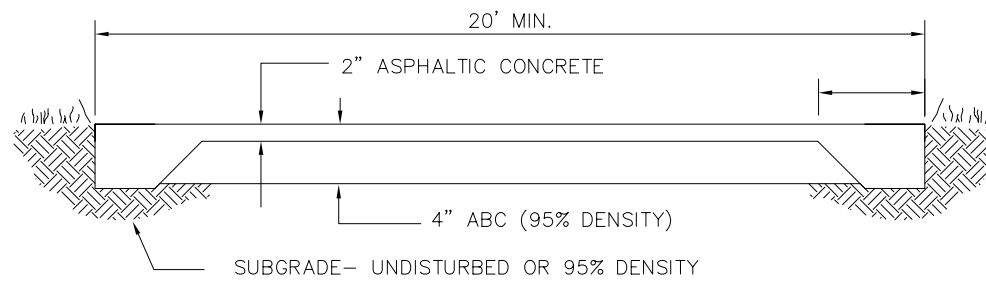
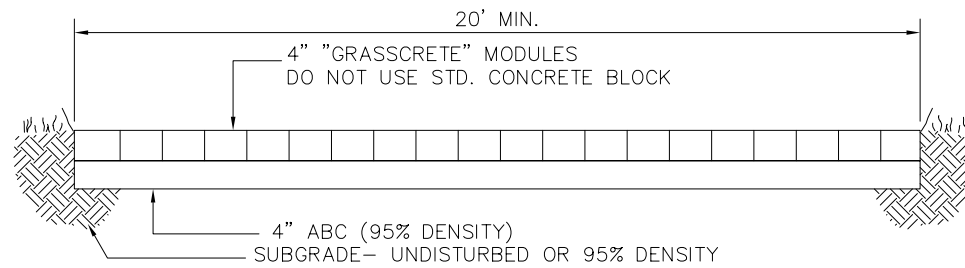
DETAIL NO.
G-3224

CITY OF GOODYEAR
STANDARD DETAIL

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MEDIAN OPENINGS FOR INTERSECTIONS

DETAIL NO.
G-3224



NOTE:

The maximum gradient of a fire department access road shall not exceed 8% (8 feet in 100 feet).

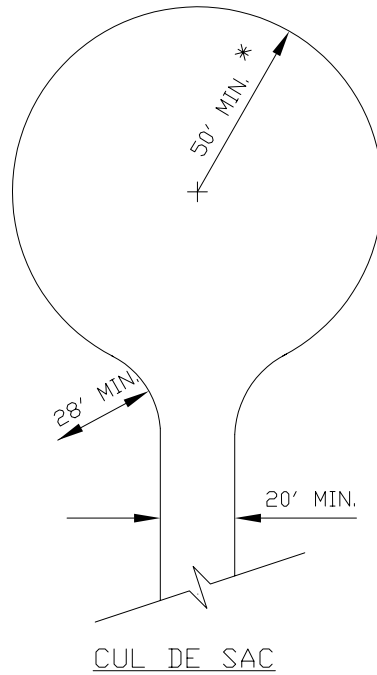
DETAIL NO.
G-3240

CITY OF GOODYEAR
STANDARD DETAIL

APPROVED BY:
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Policies Committee 7/97

FIRE DEPT. ACCESS ROADWAY
TYPICAL CROSS SECTION

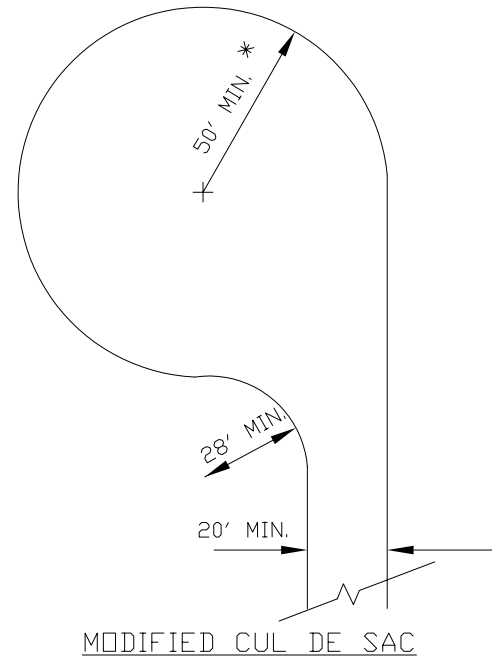
DETAIL NO.
G-3240



* NOTE:

45 FT. MINIMUM CUL-DE-SAC
RADIUS MAY BE USED FOR:

1. CUL-DE-SACS LESS THAN 150 FT.
IN LENGTH
2. CUL-DE-SACS WHERE ON STREET
PARKING IS PROHIBITED.



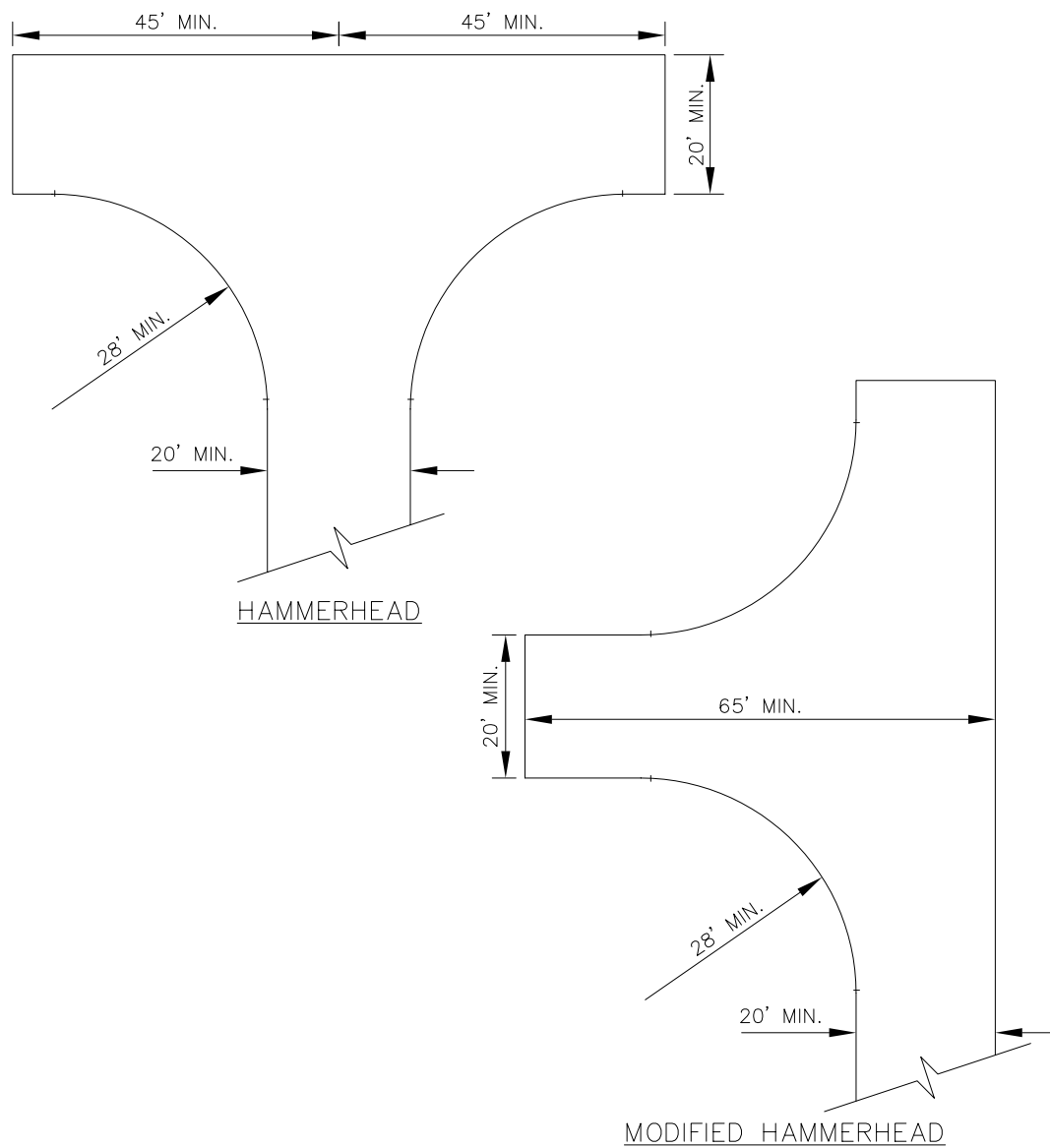
DETAIL NO.
G-3241

CITY OF GOODYEAR
STANDARD DETAIL

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Policies Committee 7/97

FIRE DEPT. ACCESS ROADWAY
CUL DE SAC / MODIFIED CUL DE SAC

DETAIL NO.
G-3241



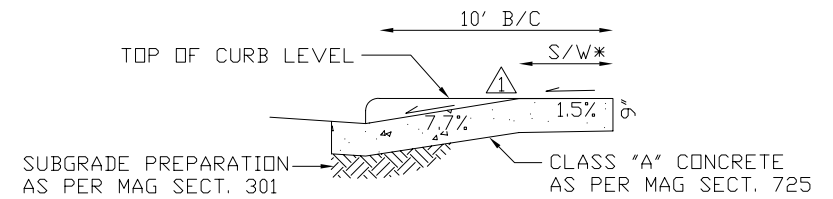
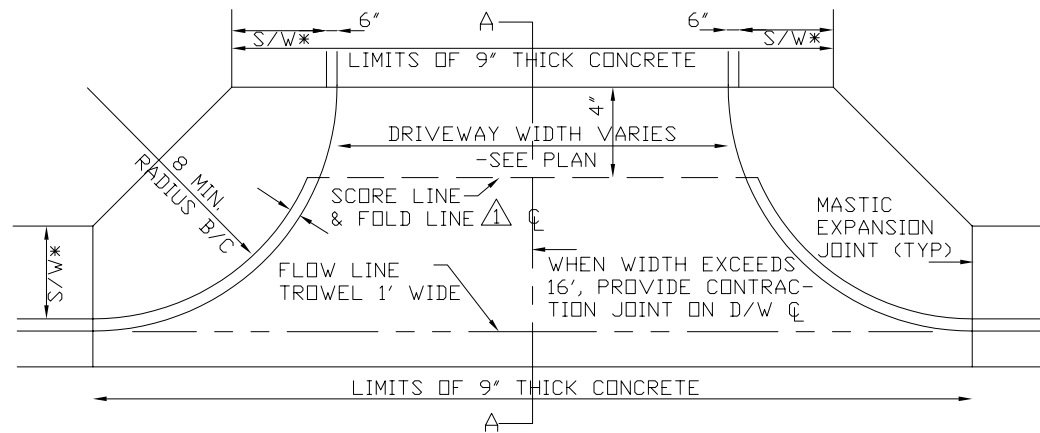
DETAIL NO.
G-3242

CITY OF GOODYEAR
STANDARD DETAIL

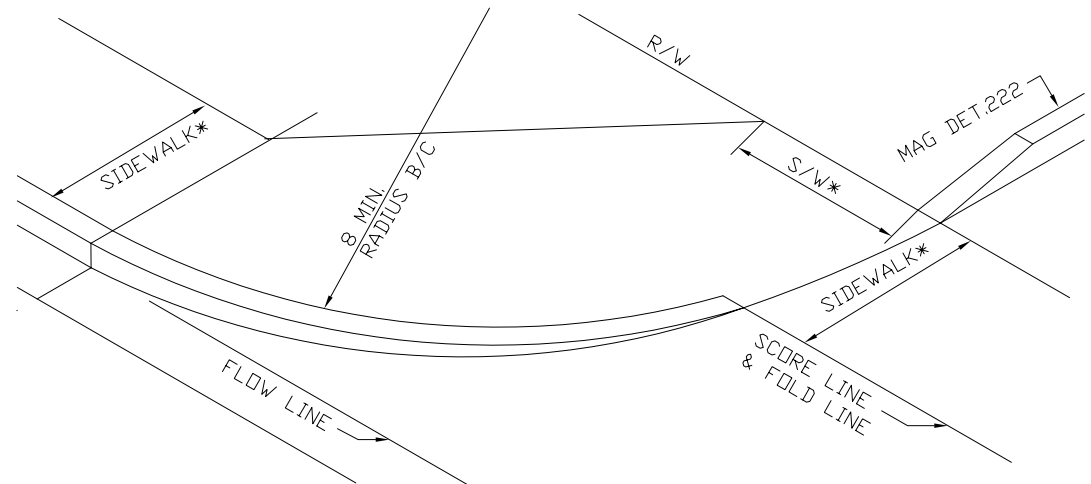
APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

FIRE DEPT. ACCESS ROADWAY
HAMMERHEAD / MODIFIED HAMMERHEAD

DETAIL NO.
G-3242



SECTION A-A



NOTE:

EXPANSION JOINT FILLER SHALL BE 1/2 INCH BITUMINOUS TYPE PREFORMED EXPANSION JOINT FILLER, A.S.T.M. D-1751.

* SIDEWALK WIDTH AS PER PLANS

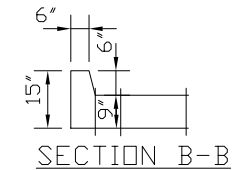
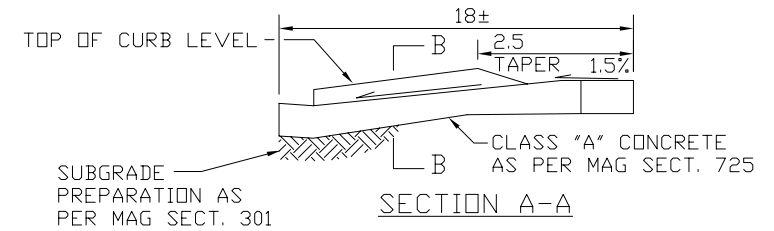
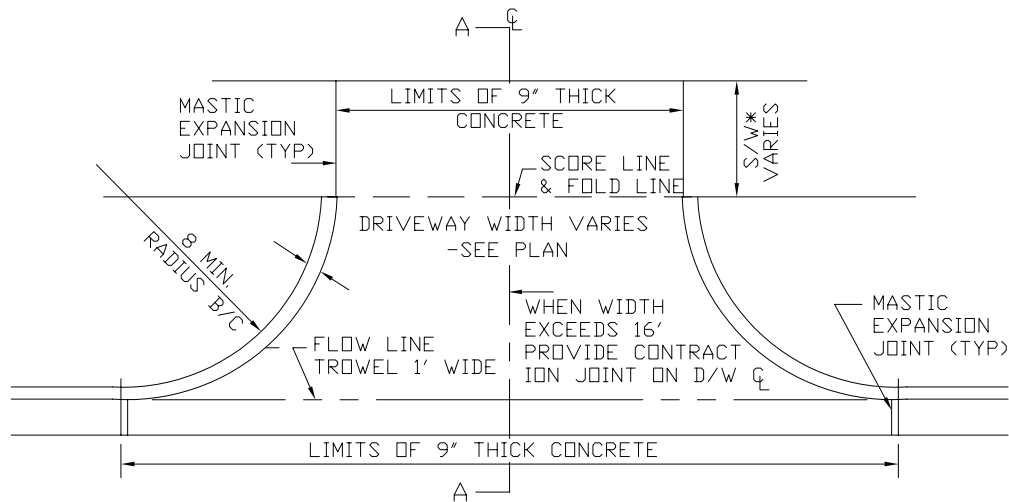
DETAIL NO.
G-3250

CITY OF GOODYEAR
STANDARD DETAIL

APPROVED BY:
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Policies Committee 7/97

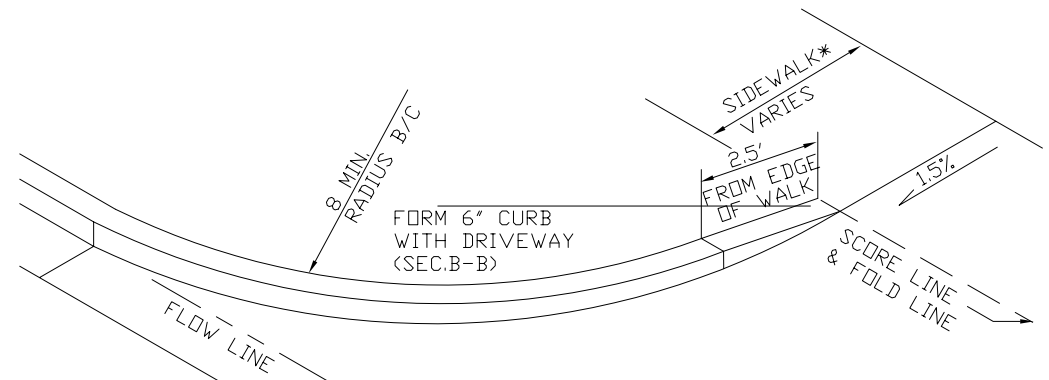
RETURN TYPE DRIVEWAYS
FOR HANDICAP ACCESS

DETAIL NO.
G-3250



NOTE:
EXPANSION JOINT FILLER SHALL BE 1/2 INCH BITUMINOUS TYPE PREFORMED EXPANSION JOINT FILLER, A.S.T.M. D-1751.

* SIDEWALK WIDTH AS PER PLANS



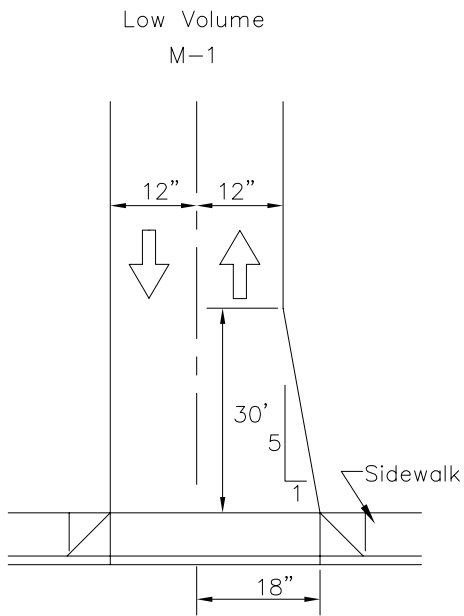
DETAIL NO.
G-3251

CITY OF GOODYEAR
STANDARD DETAIL

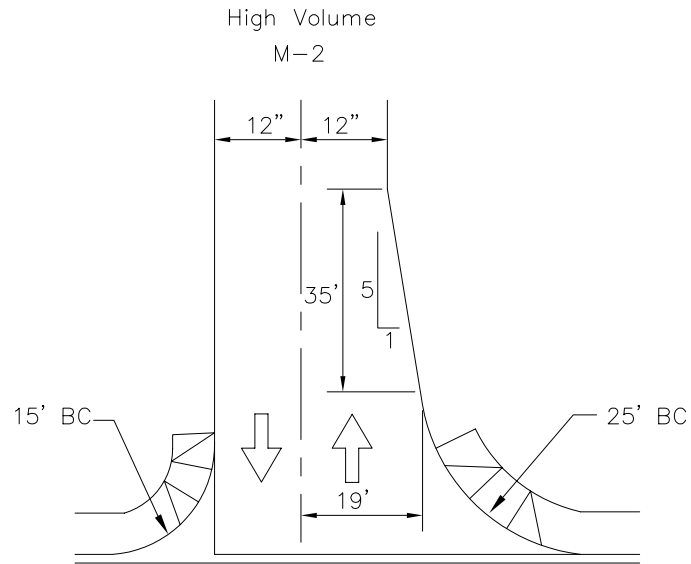
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RETURN TYPE DRIVEWAYS WITH
DETACHED SIDEWALK

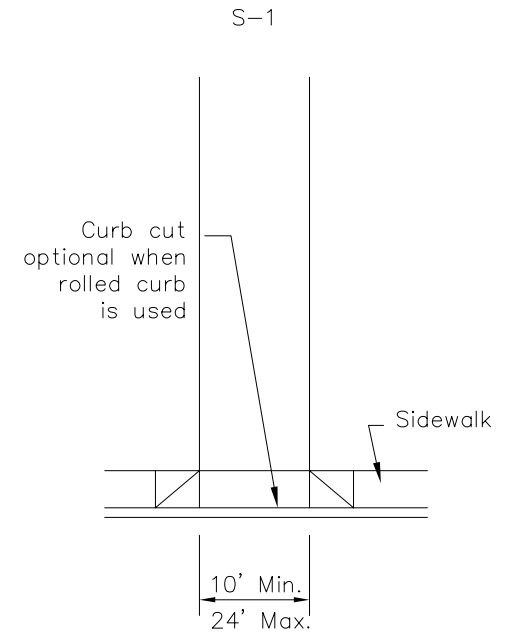
DETAIL NO.
G-3251



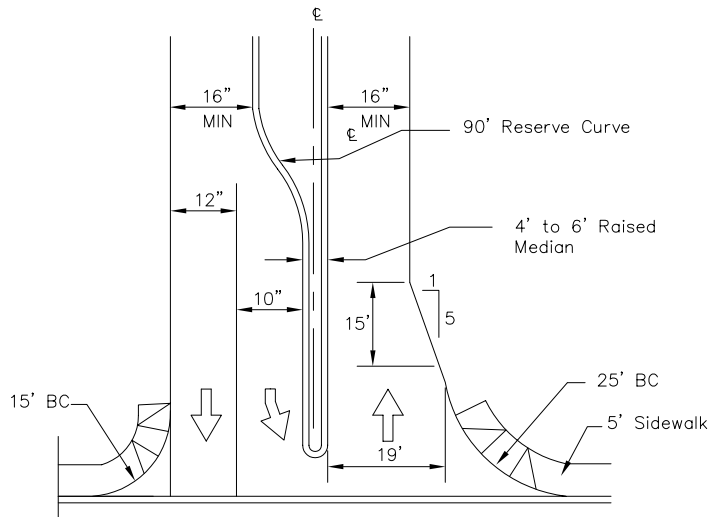
Multi-Family Units



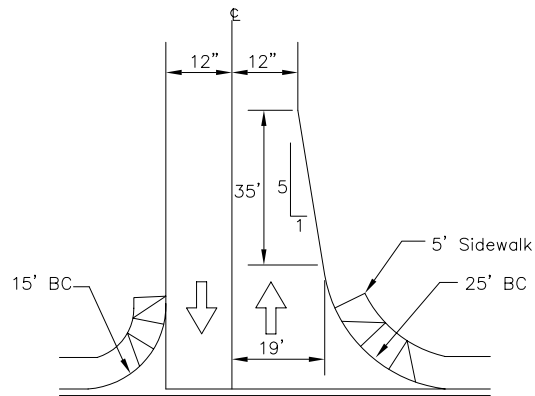
Multi-Family Units



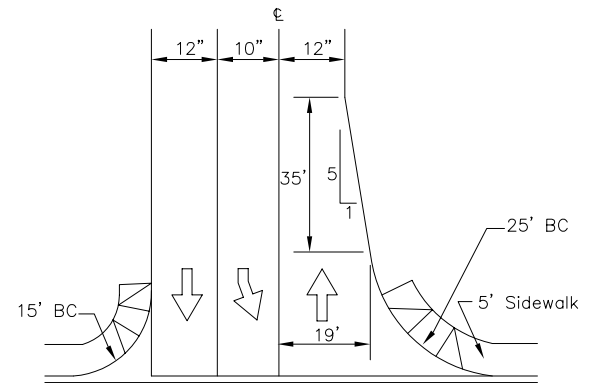
Single Family Unit



Two Way with Two Egress Lanes
CH-2



Two Way
CH-1



Two Way with Two Egress Lanes
CH-2

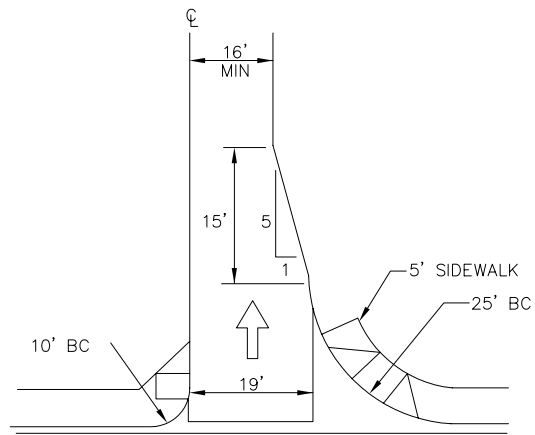
DETAIL NO.
G-3254-2

CITY OF GOODYEAR
STANDARD DETAIL

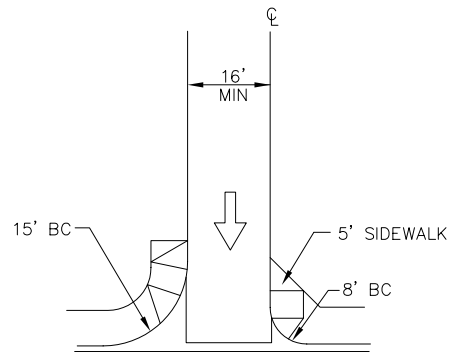
APPROVED BY:
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Policies Committee
7/97

COMMERCIAL/INDUSTRIAL DRIVEWAYS - TYPE CH

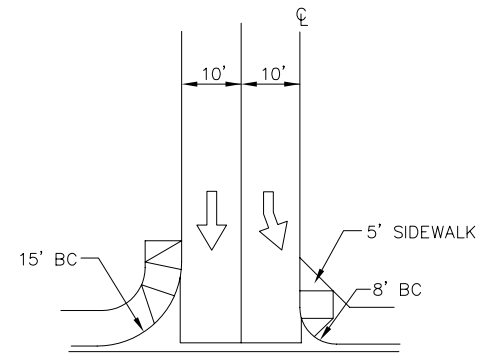
DETAIL NO.
G-3254-2



ONE WAY INGRESS
CI-1



ONE WAY INGRESS
CI-2



TWO LANES, ONE WAY EGRESS
CI-3

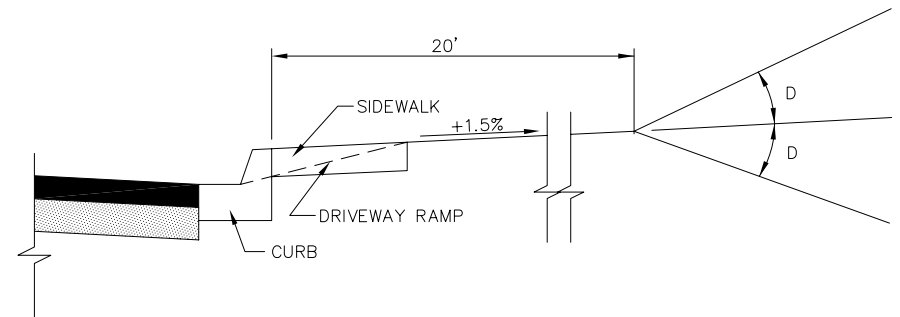
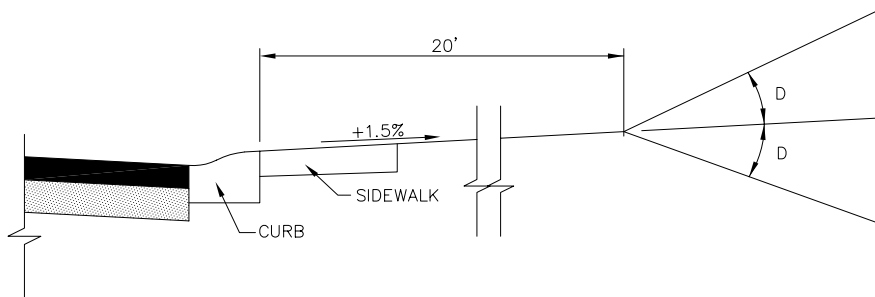
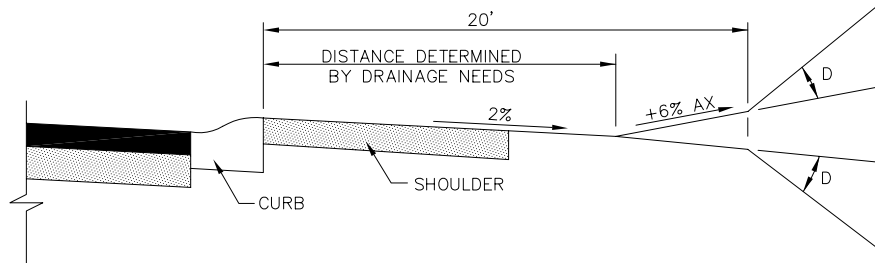
DETAIL NO.
G-3254-3

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STANDARD DETAIL

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COMMERCIAL/INDUSTRIAL DRIVEWAYS - TYPE CI

DETAIL NO.
G-3254-3



A.D.T. FOR DRIVEWAY

GRADE DIFFERENCE, D

		DESIRABLE	MAXIMUM
LOW VOLUME	1-500	6%	10%
MEDIUM VOLUME	500-1500	3%	10%
HIGH VOLUME	1500 OR MORE	0%	10%

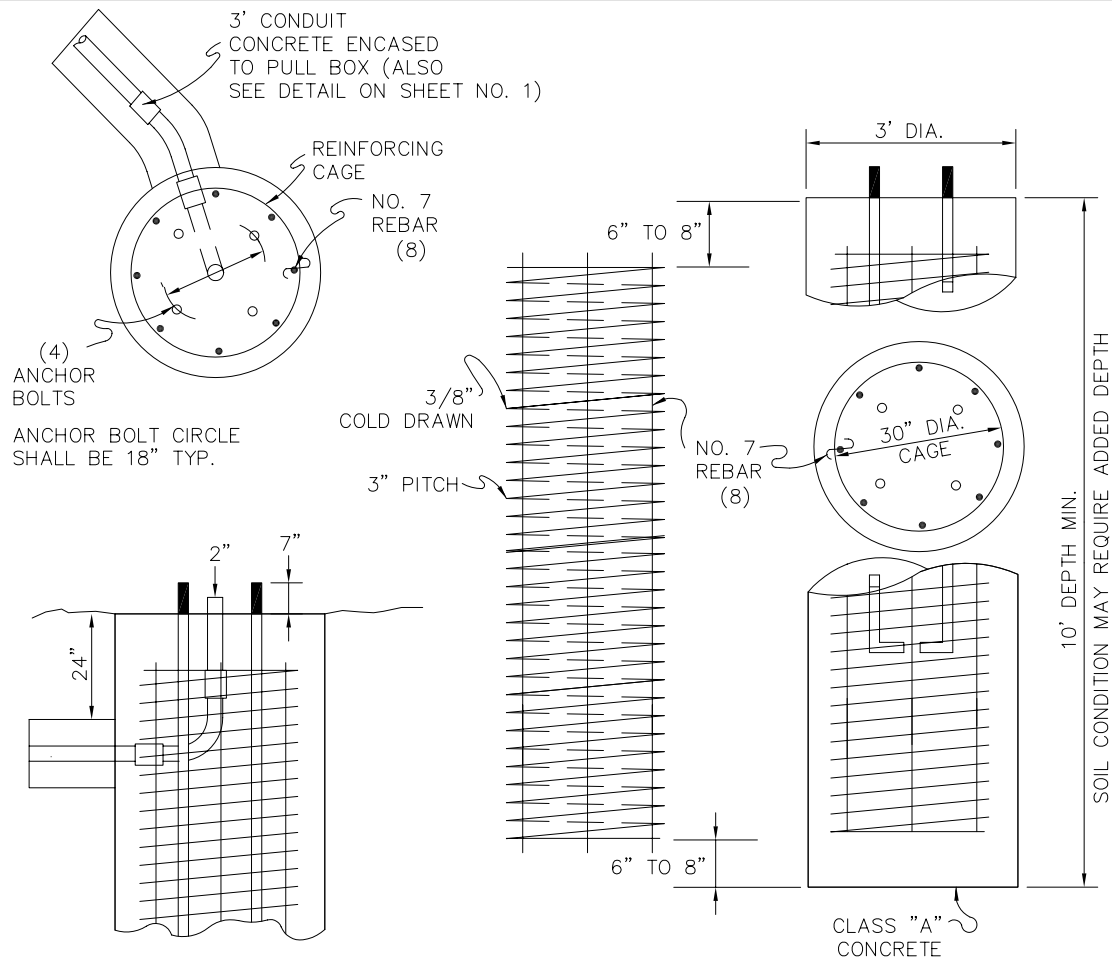
DETAIL NO.
G-3258

CITY OF GOODYEAR
STANDARD DETAIL

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NON-RESIDENTIAL DRIVEWAY GRADE STANDARDS

DETAIL NO.
G-3258



FOUNDATION SHALL BE POURED IN
DRILLED HOLE PER A.D.O.T. STD.
DRW. NO. T.S. 4-10,11,12, OR 13.

ANCHOR BOLTS & CONDUIT
WILL REQUIRE INTERIM PROTECTION
AFTER BEING INSTALLED. SEE
STD. DETAIL G-129

ANCHOR BOLTS SHALL BE GALVANIZED
2'x 90" WITH L BENDS AND BE
PROVIDED WITH (2) HEX NUTS AND
(2) WASHERS EACH.

ADOT REFERENCE T.S. 4-10

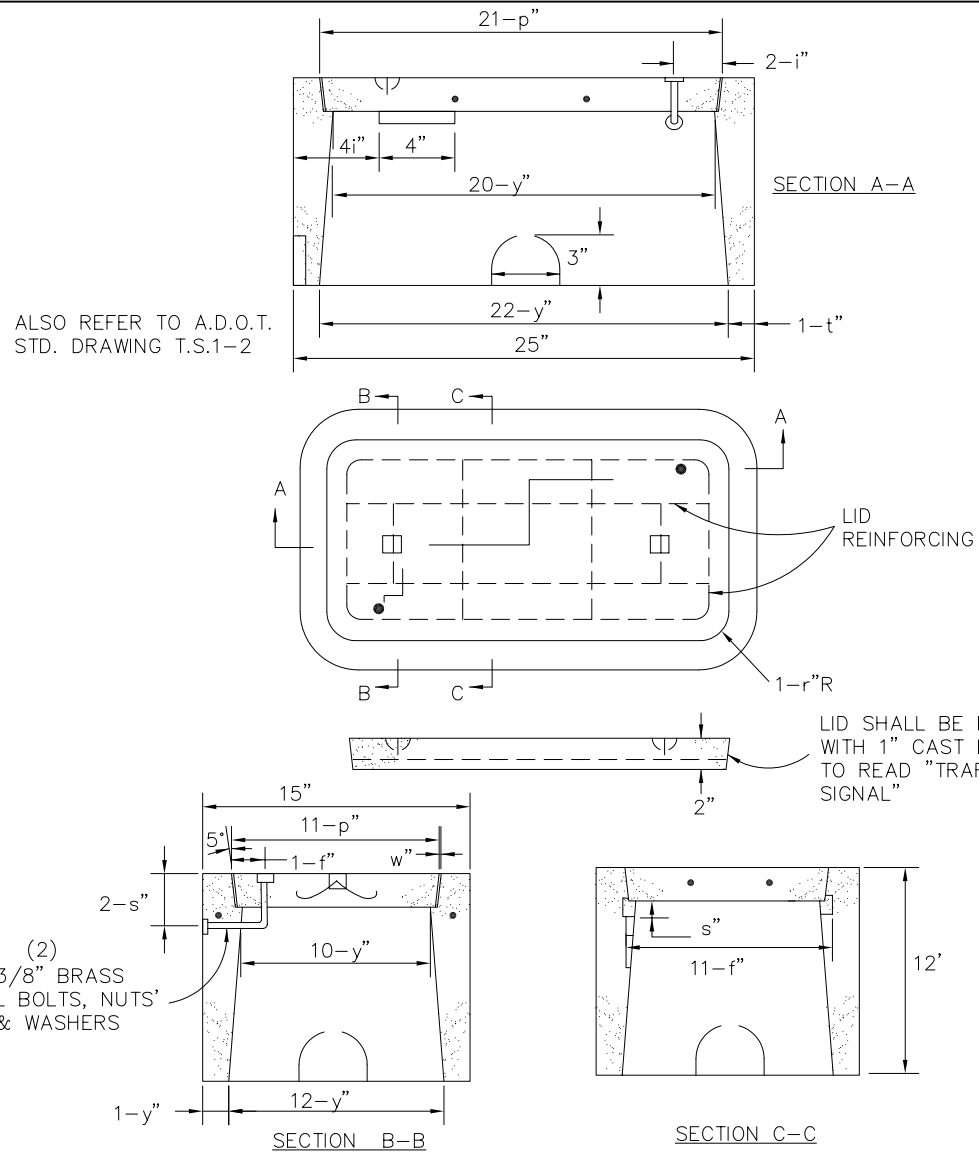
DETAIL NO.
G-3260

CITY OF GOODYEAR
STANDARD DETAIL

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SIGNAL POLE STANDARD
ANCHOR BASE DETAIL

DETAIL NO.
G-3260



DETAIL NO.
G-3261

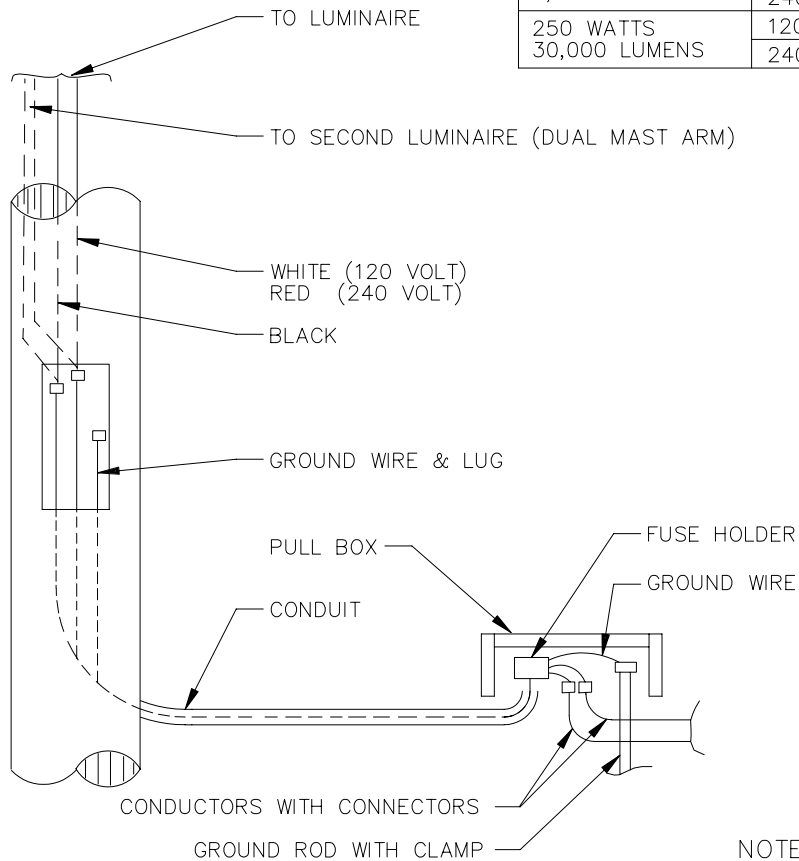
CITY OF GOODYEAR
STANDARD DETAIL

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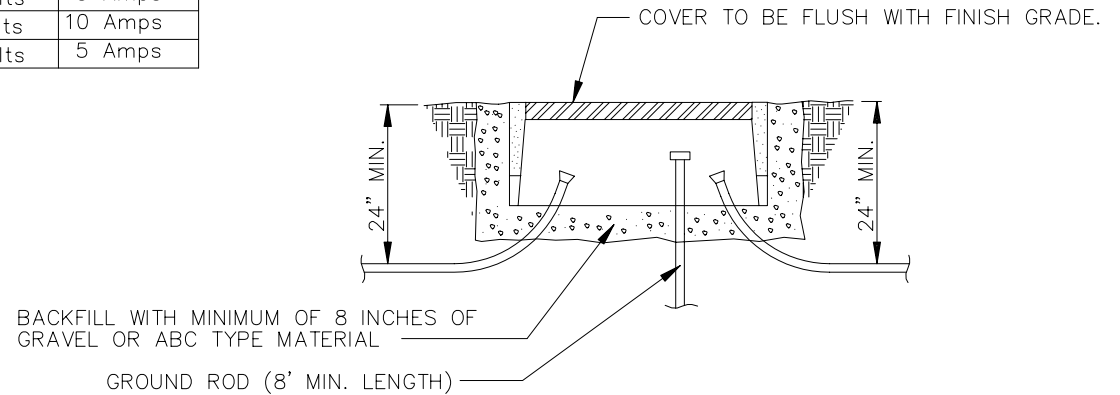
SIGNAL EQUIPMENT STANDARD
TYPE 5 PULL BOX

DETAIL NO.
G-3261

LAMP	VOLTAGE	FUSE SIZE
100 WATTS 9,500 LUMENS	120 Volts	8 Amps
	240 Volts	5 Amps
250 WATTS 30,000 LUMENS	120 Volts	10 Amps
	240 Volts	5 Amps



CONNECTION DETAIL



PULL BOX DETAIL



COVER DETAIL

NOTES:

1. ALL PULL BOXES SHALL BE A.D.O.T. TYPE 3-1/2, OR APPROVED EQUAL.
2. ALL PULL BOXES SHALL BE PROVIDED BY ARIZONA PUBLIC SERVICES.

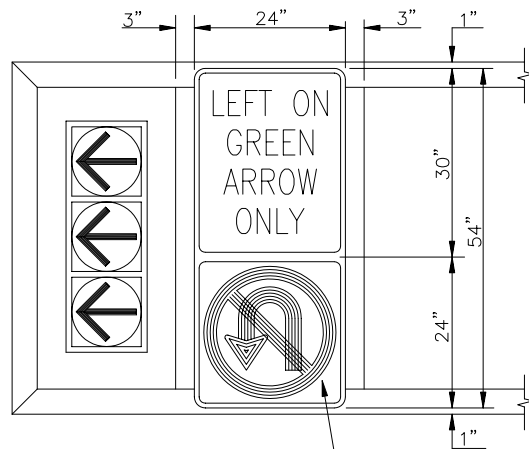
DETAIL NO.
G-3263

**CITY OF GOODYEAR
STANDARD DETAIL**

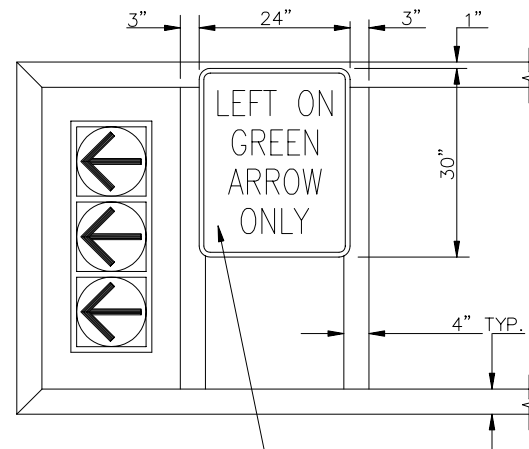
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STREET LIGHT PULL BOX & CONNECTION DETAILS

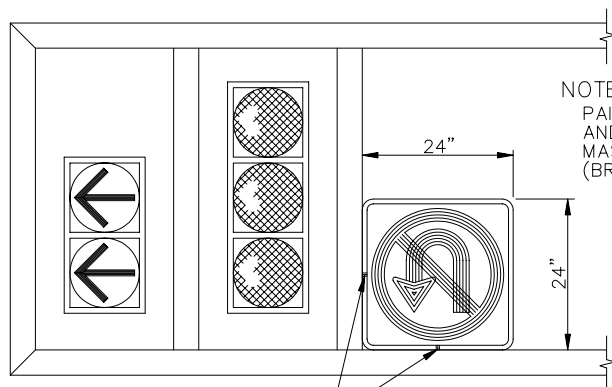
DETAIL NO.
G-3263



MOUNT DUAL SIGN ON SINGLE UNIT, CENTER OVER BLANK PANEL ON SIGNAL MAST ARM



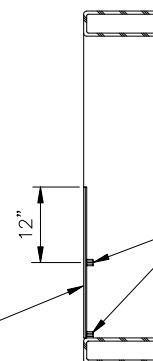
MOUNT SINGLE SIGN, CENTER OVER BLANK PANEL ON SIGNAL MAST ARM



NOTE:
PAINT BACK SIDES OF SIGN
AND ANGLE IRON TO MATCH
MAST ARM COLOR.
(BRONZE)

1" x 1" x 1" ANGLE IRON TABS WELDED TO MAST
ARM FOR SIGN MOUNTING. (SIGN FACE TO BE
FLUSH WITH APPROACH SIDE OF MAST ARM)

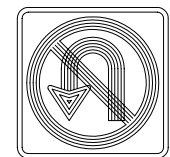
SIGN FACE



SECTION



R10-5z



R3-4

DETAIL NO.
G-3265

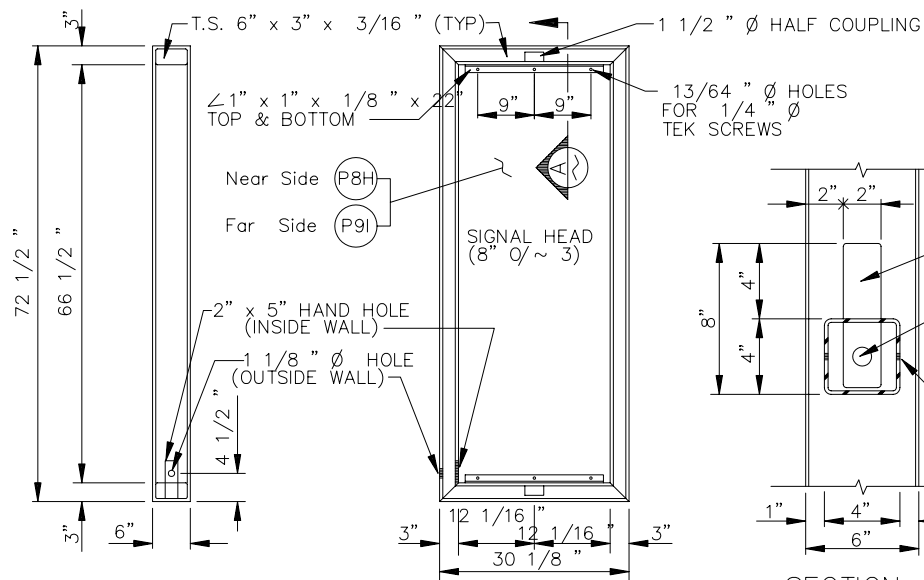
CITY OF GOODYEAR
STANDARD DETAIL

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MODULAR SIGNAL MAST ARM SIGN MOUNTING STANDARDS

DETAIL NO.
G-3265

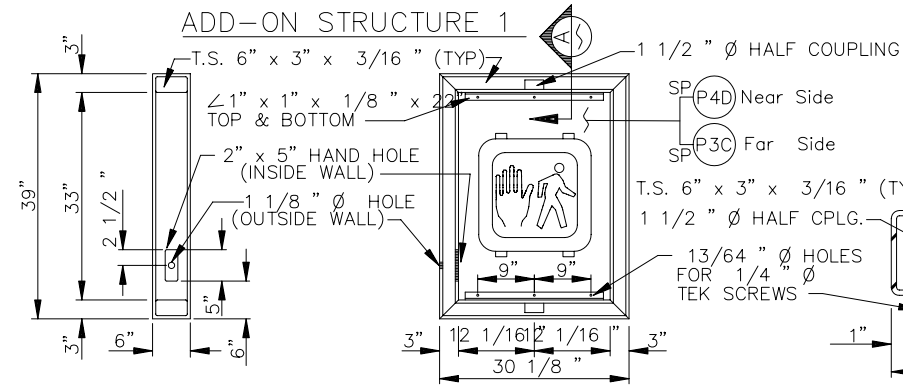


SIDE

FRONT

ADD-ON STRUCTURE 1

SECTION (B)

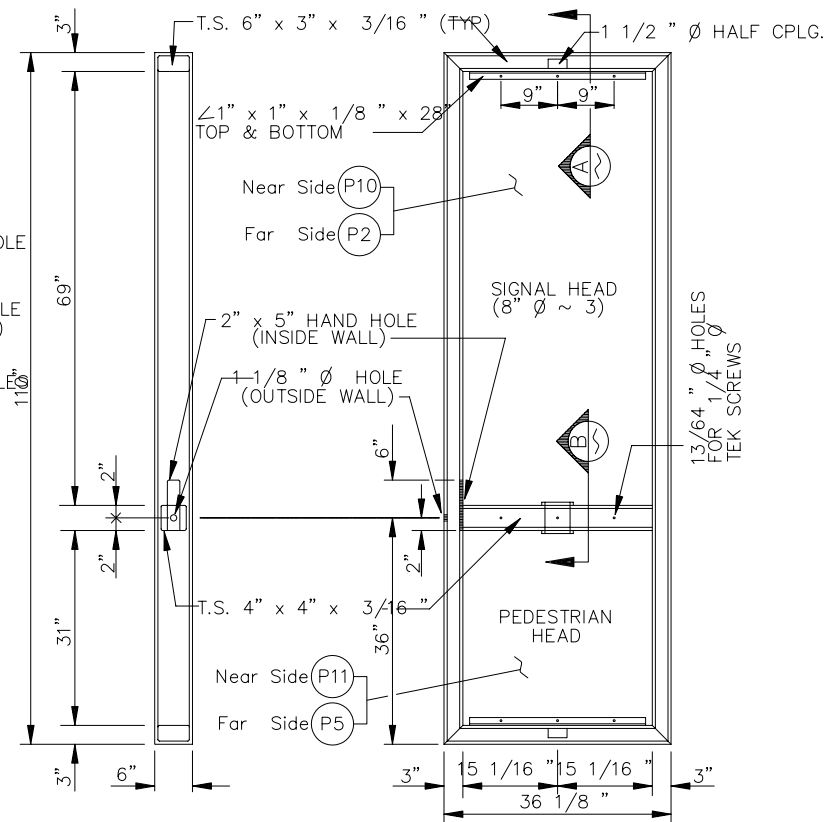


SIDE

FRONT

ADD-ON STRUCTURE 2

TYPICAL SECTION (A)



SIDE

FRONT

ADD-ON STRUCTURE 3

DETAIL NO.
G-3266

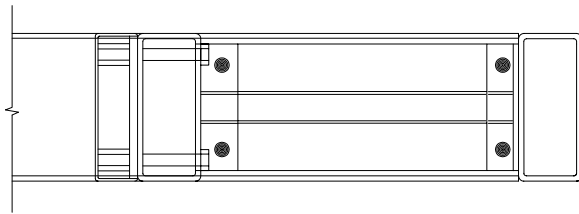
CITY OF GOODYEAR
STANDARD DETAIL

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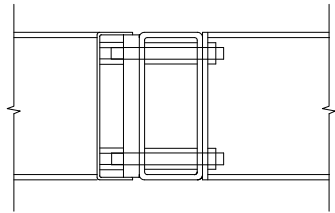
7/97

MODULAR SIGNAL ADD-ON STRUCTURES

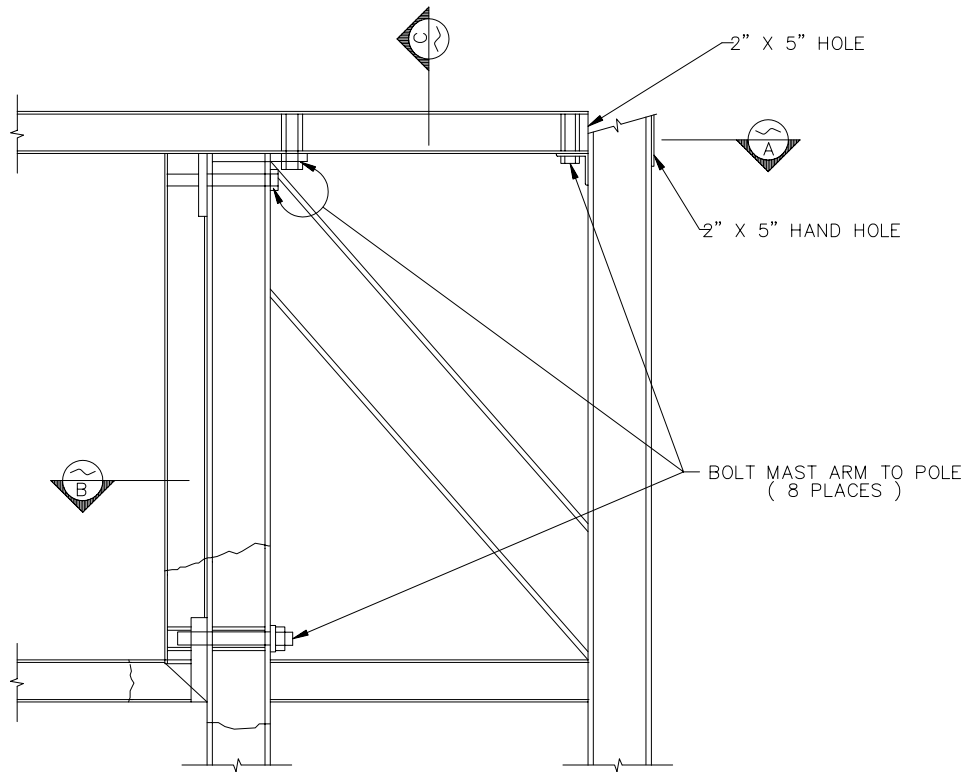
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G-3266



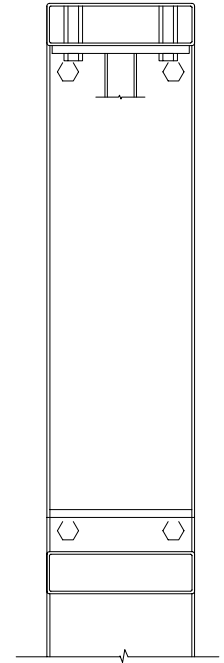
SECTION A



SECTION B



MAST ARM CONNECTION DETAIL



SECTION C

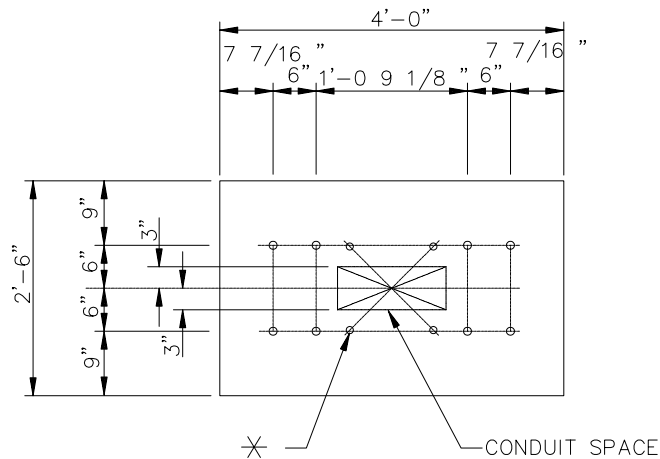
DETAIL NO.
G-3267

CITY OF GOODYEAR
STANDARD DETAIL

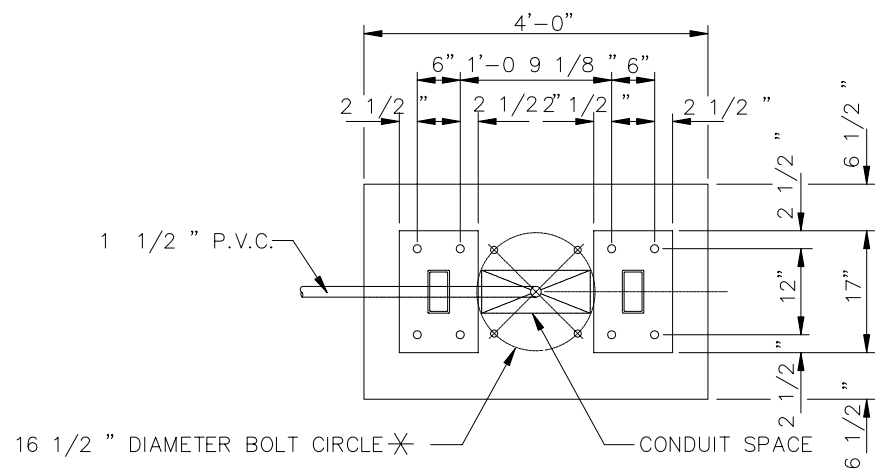
APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

MODULAR POLE MAST ARM CONNECTION

DETAIL NO.
G-3267



ANCHOR BOLT LAYOUT PLAN



BASE PLATE LAYOUT PLAN

GENERAL NOTES

1. CONCRETE 4000 P.S.I. @ 28 DAYS
2. REBAR GRADE 60
3. ANCHOR BOLTS A-36 FULLY GALVANIZED
4. EXISTING SOIL CONDITIONS TO BE DETERMINED PRIOR TO FINAL FOUNDATION DESIGN.
5. A 25' COIL OF NO. 4 STRANDED A.W.G. BARE COPPER CONDUCTOR SHALL BE INSTALLED BEFORE THE CONCRETE IS POURED.
6. ADDITIONAL 1 1/2" P.V.C. CONDUIT MAY BE REQUIRED FOR LOOPS, SEE SIGNAL PLAN.

* BOLTS FOR INTERIM SIGNAL ERECTION MAY BE DELETED AT ENGINEER'S DIRECTION.

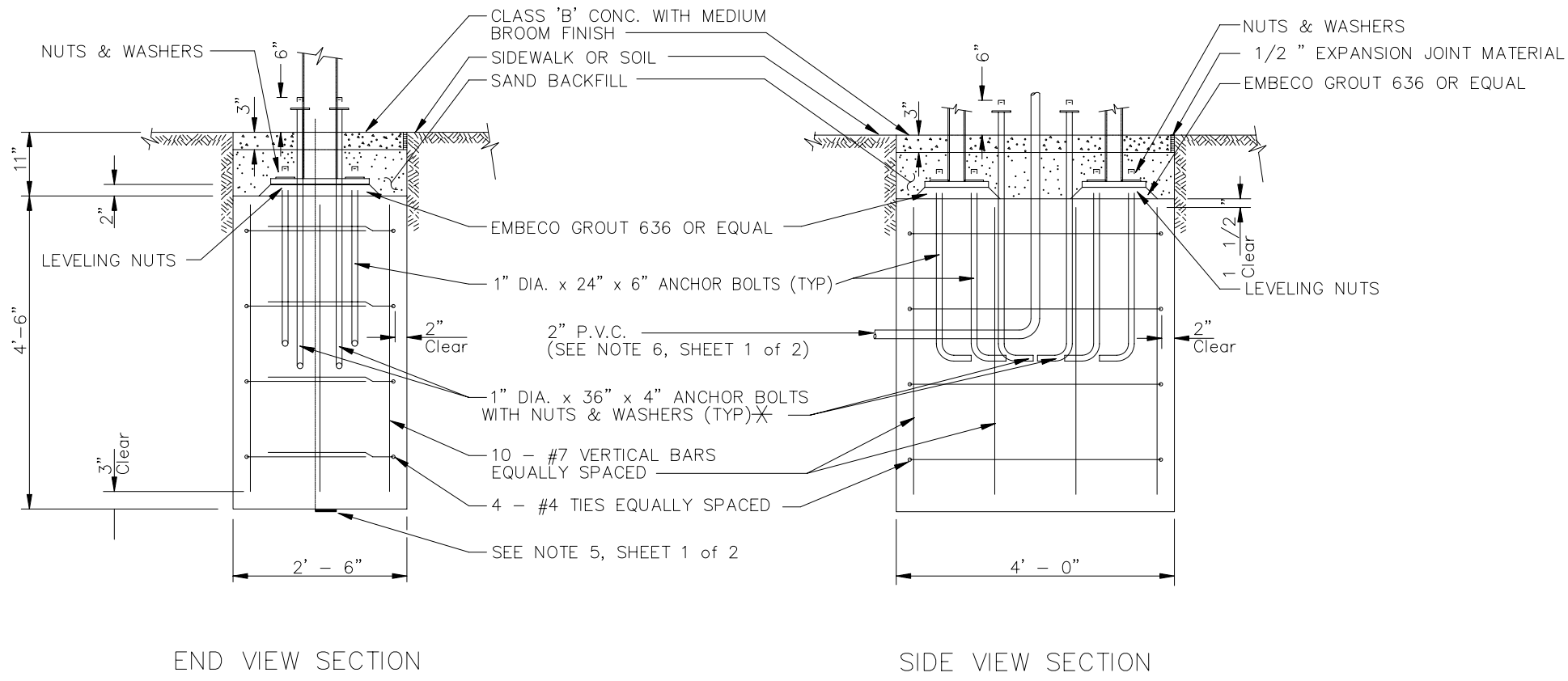
DETAIL NO.
G-3270-1

CITY OF GOODYEAR
STANDARD DETAIL

APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

TRAFFIC SIGNAL FOUNDATION DETAIL
FOUNDATION FOR TYPE "A" MODULAR (NO MAST ARM)

DETAIL NO.
G-3270-1



* BOLTS FOR INTERIM SIGNAL ERECTION
MAY BE DELETED AT ENGINEER'S DIRECTION.

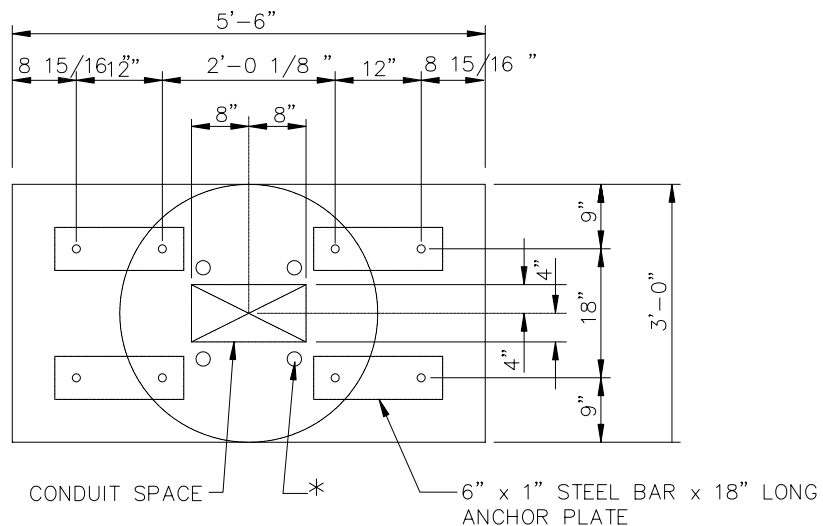
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**CITY OF GOODYEAR
STANDARD DETAIL**

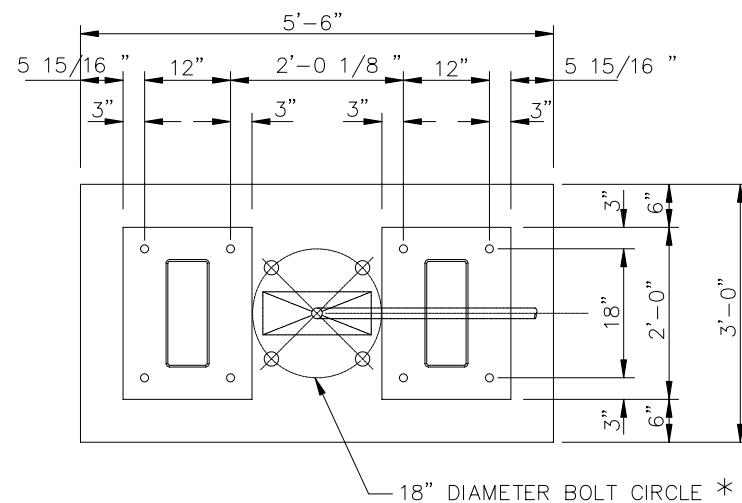
APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

**TRAFFIC SIGNAL FOUNDATION DETAIL
FOUNDATION FOR TYPE "A" MODULAR (NO MAST ARM)**

DETAIL NO.
G-3270-2



ANCHOR BOLT LAYOUT PLAN



BASE PLATE LAYOUT PLAN

GENERAL NOTES

1. MINIMUM SOIL REQUIREMENTS:
THIS FOUNDATION DESIGN IS BASED ON SOILS ABLE TO DEVELOP THE FOLLOWING VALUES FOR CONCRETE FILLED DRILLED IN PLACE PIERS, SKIN FRICTION AT 500 LBS/SQ. FT., LATERAL BEARING PRESSURE = 200 LBS/ SQ. FT. PER FOOT OF DEPTH.
2. EXISTING SOIL CONDITIONS TO BE DETERMINED PRIOR TO FINAL FOUNDATION DESIGN.
3. CONCRETE 4000 P.S.I. AT 28 DAYS.
4. REBAR GRADE 60.
5. EMBEDDED PLATES - A-36.
6. ANCHOR BOLTS - A-36 FULLY GALVANIZED.
7. A 25' COIL OF NO. 4 STRANDED A.W.G. BARE COPPER CONDUCTOR SHALL BE INSTALLED BEFORE THE CONCRETE IS POURED.

*BOLTS FOR INTERIM SIGNAL ERECTION
MAY BE DELETED AT ENGINEER'S DIRECTION.

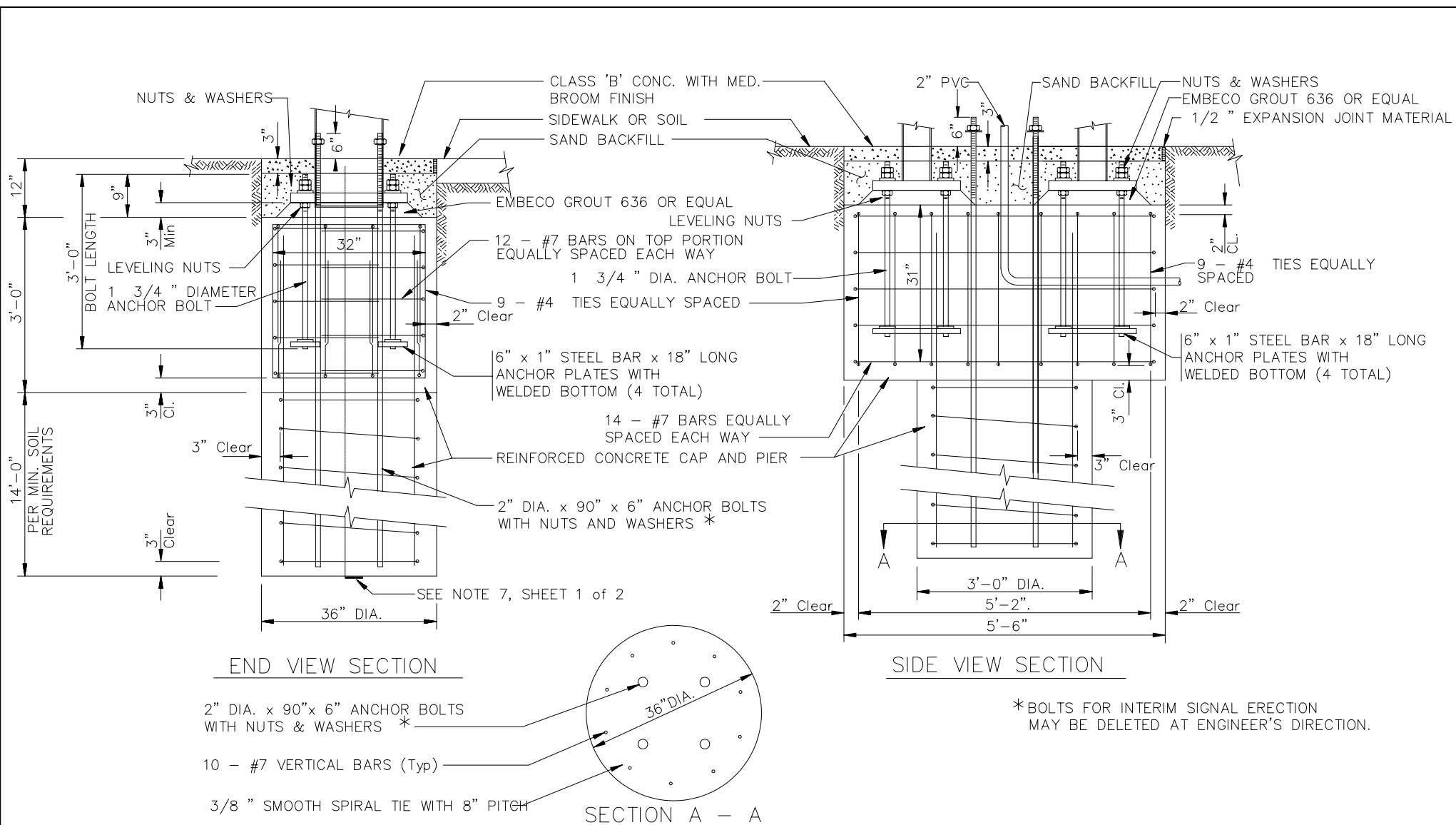
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CITY OF GOODYEAR
STANDARD DETAIL

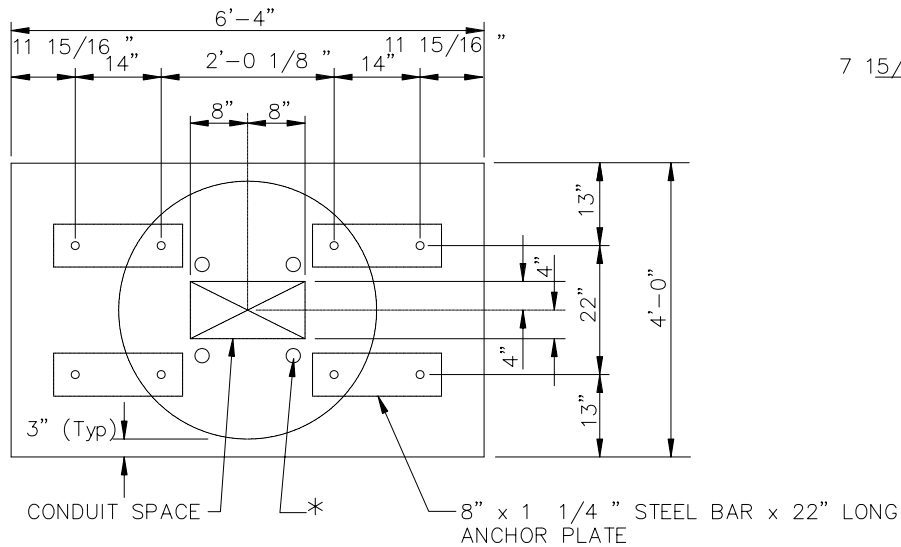
APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

TRAFFIC SIGNAL FOUNDATION DETAIL
FOUNDATION FOR MODULAR 20, 35, 40 MAST ARM STRUCTURES

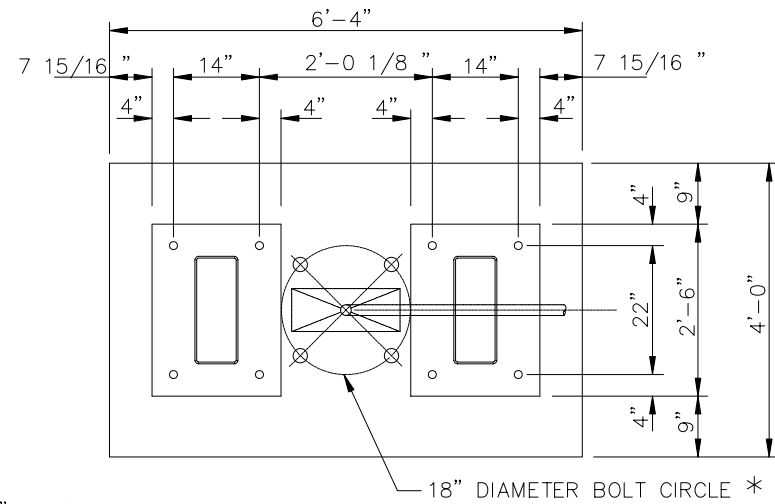
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G-3271-1



DETAIL NO. G-3271-2	CITY OF GOODYEAR STANDARD DETAIL	APPROVED BY: Goodyear Standards and Policies Committee 7/97	TRAFFIC SIGNAL FOUNDATION DETAIL FOUNDATION FOR MODULAR 20, 35, 40 MAST ARM STRUCTURES	DETAIL NO. G-3271-2
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ANCHOR BOLT LAYOUT PLAN



BASE PLATE LAYOUT PLAN

GENERAL NOTES

1. MINIMUM SOIL REQUIREMENTS:
THIS FOUNDATION DESIGN IS BASED ON SOILS ABLE TO DEVELOP THE FOLLOWING VALUES FOR CONCRETE FILLED DRILLED IN PLACE PIERS.
SKIN FRICTION AT 500 LBS/SQ. FT., LATERAL BEARING PRESSURE = 200 LBS/SQ. FT. PER FOOT OF DEPTH.
2. EXISTING SOIL CONDITIONS TO BE DETERMINED PRIOR TO FINAL FOUNDATION DESIGN.
3. CONCRETE 4000 P.S.I. AT 28 DAYS.
4. REBAR GRADE 60.
5. EMBEDDED PLATES - A-36.
6. ANCHOR BOLTS - A-36 FULLY GALVANIZED.
7. A 25' COIL OF NO. 4 STRANDED A.W.G. BARE COPPER CONDUCTOR SHALL BE INSTALLED BEFORE THE CONCRETE IS POURED.

*BOLTS FOR INTERIM SIGNAL ERECTION
MAY BE DELETED AT ENGINEER'S DIRECTION.

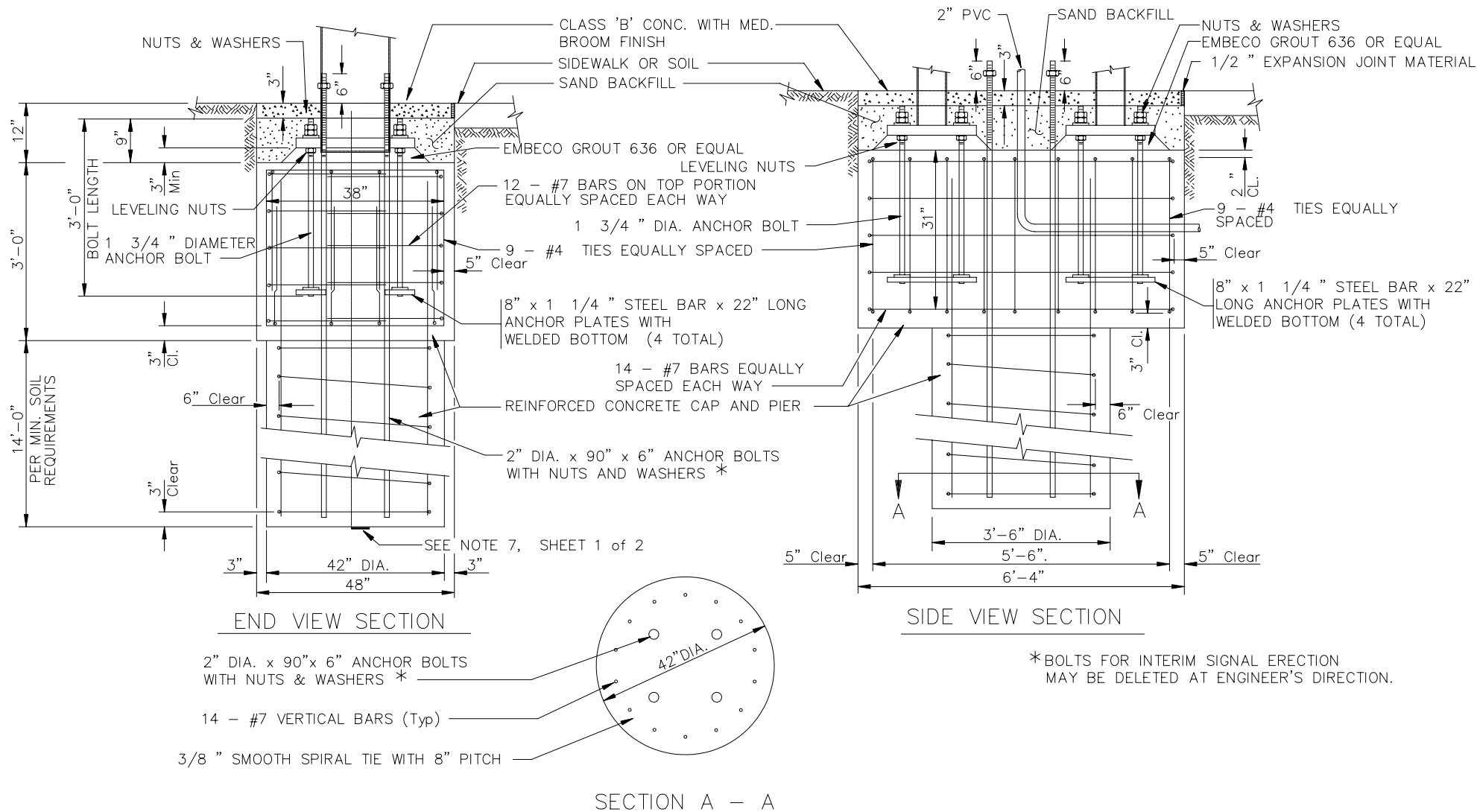
DETAIL NO.
G-3272-1

CITY OF GOODYEAR
STANDARD DETAIL

APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

TRAFFIC SIGNAL FOUNDATION DETAIL
FOUNDATION FOR MODULAR 45 MAST ARM STRUCTURE

DETAIL NO.
G-3272-1



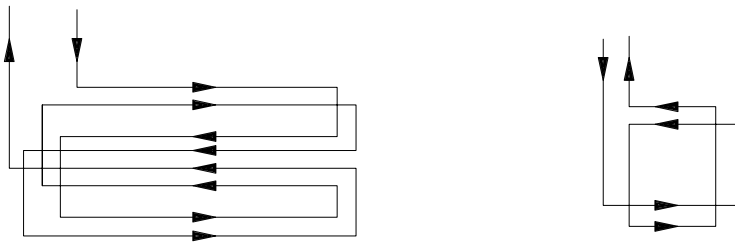
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CITY OF GOODYEAR
STANDARD DETAIL

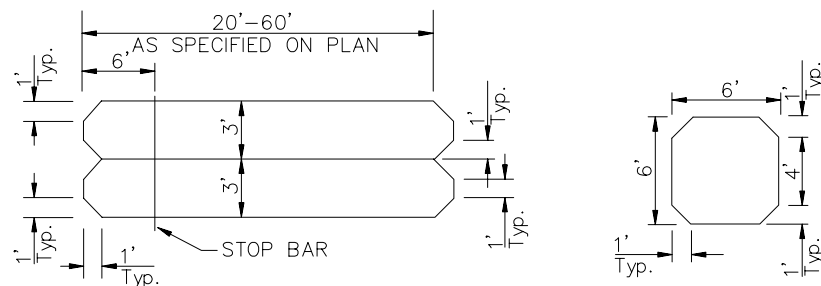
APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

TRAFFIC SIGNAL FOUNDATION DETAIL
FOUNDATION FOR MODULAR 45 MAST ARM STRUCTURE

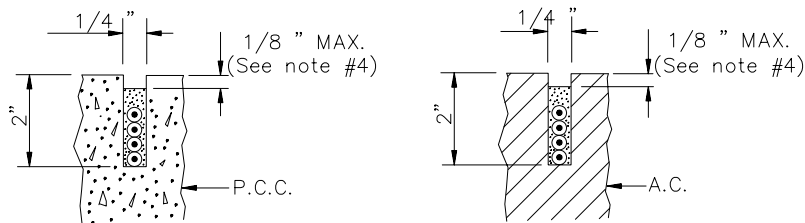
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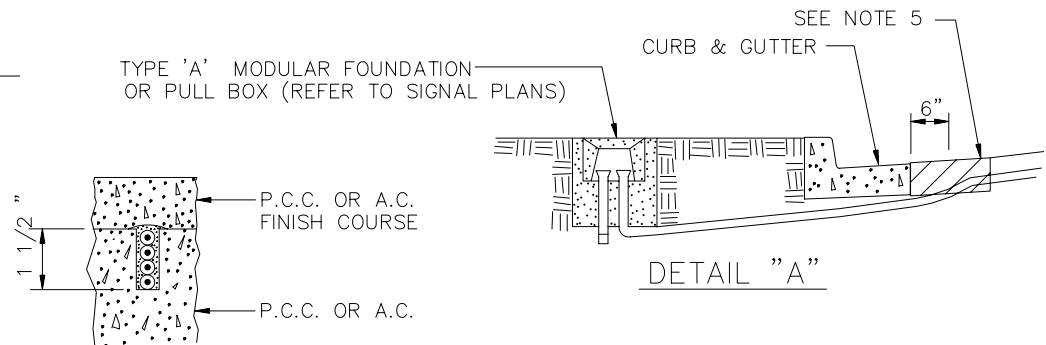
WIRING DIAGRAM FOR OCCUPANCY LOOP DETECTOR
LOOP DUCT (ORANGE)



OCCUPANCY LOOP DETECTOR SAW CUT PATTERN



LOOPS IN FINISH COURSE



LOOPS IN SUB-BASE

NOTES:

1. CUT THE DIAGONALS AS SHOWN TO PREVENT SHARP BENDS IN THE WIRE. OVERCUT THE DIAGONALS SO THAT THE CORNERS HAVE THE FULL DEPTH REQUIRED.
2. THE SAW CUT SHALL BE AS SHOWN UNLESS OTHERWISE NOTED.
3. BLOW OUT ALL SAW CUTS BEFORE INSERTING THE WIRES. WIRES SHALL BE INSERTED IN SUCH A MANNER THAT THE INSULATION SHALL NOT BE DAMAGED.
4. SAW CUTS SHALL BE FILLED WITH EPOXY LOOP SEALANT, OR EQUIVALENT SEALANT AS APPROVED BY CITY ENGINEER.
5. USE SAME MATERIAL (OR APPROVED EQUAL) FOR PATCHING EXISTING PAVEMENT. PATCH TO AT LEAST 1/4 " HIGHER THAN SURFACE OF EXISTING PAVEMENT.
6. ALL DETECTOR LOOPS SHALL BE GIVEN A CONTINUITY AND INSULATION TEST BEFORE AND AFTER PLACING THE FINAL PAVING OR PLACING THE SEALER IN THE SAW CUTS.
7. LOOP DETECTORS SHALL BE LOCATED IN CENTER OF TRAVELED LANE UNLESS OTHERWISE NOTED ON PLANS AND SHALL BE APPROVED PRIOR TO SAW CUTTING.
8. LEFT-TURN LANE DETECTOR LEAD-IN SHALL BE INSTALLED IN A SEPARATE SAW CUT.
9. NO MORE THAN TWO ADJACENT DETECTOR LEAD-INS SHALL BE IN THE SAME SAW CUT.
10. DETECTOR LEAD-IN SAW CUTS SHALL BE 1' APART.

TYPE 'A' MODULAR FOUNDATION
OR PULL BOX (REFER TO SIGNAL PLANS)

CURB & GUTTER

SEE NOTE 5

DETAIL "A"

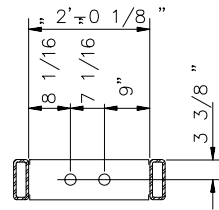
DETAIL NO.
G-3275

CITY OF GOODYEAR
STANDARD DETAIL

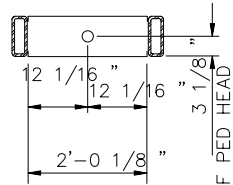
APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

TRAFFIC LOOP DETECTOR DETAIL

DETAIL NO.
G-3275

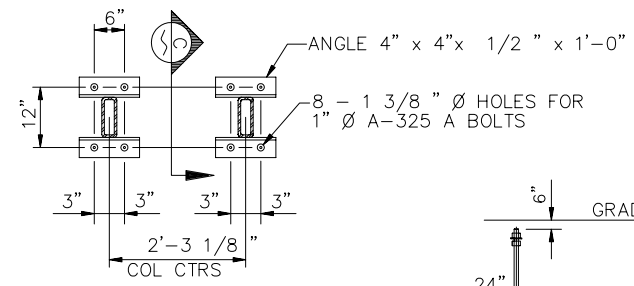


SECTION A

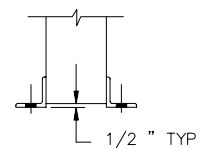


SECTION B

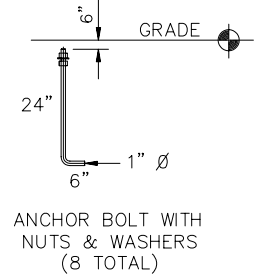
*PANELS & PANEL CUTOUTS SHALL BE DETERMINED BY INTERSECTION DESIGN



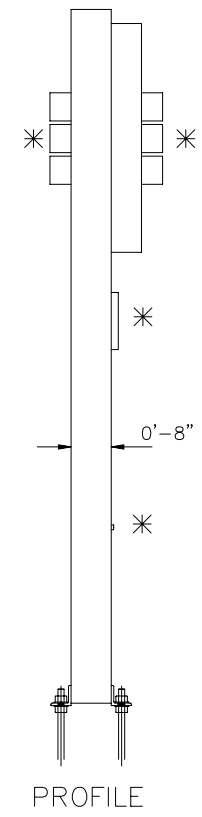
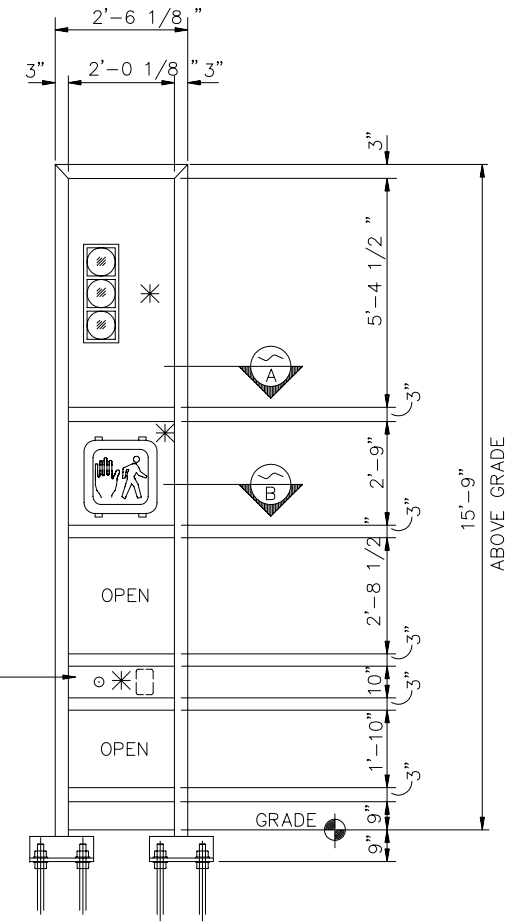
ANCHOR BOLT SETTING PLAN



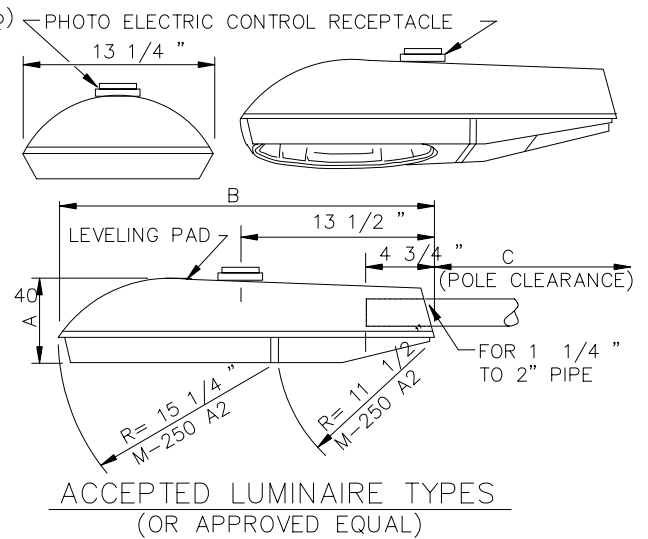
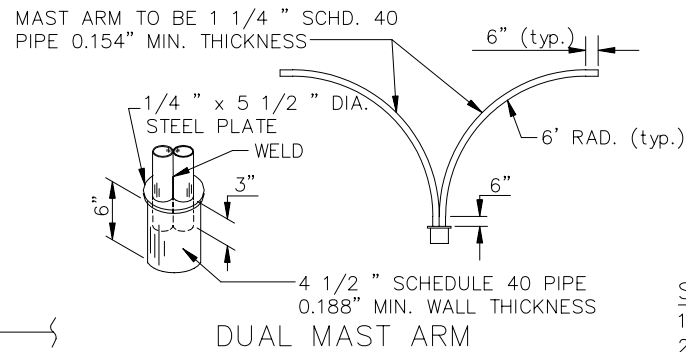
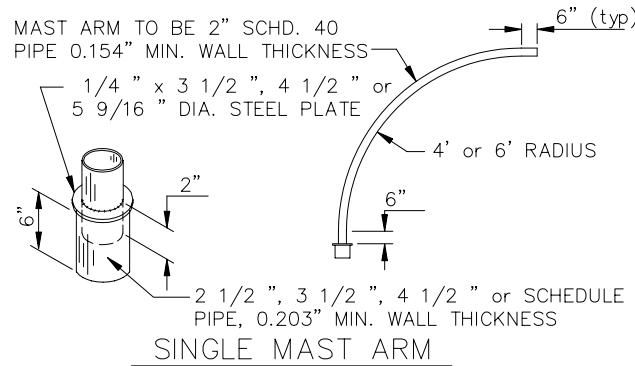
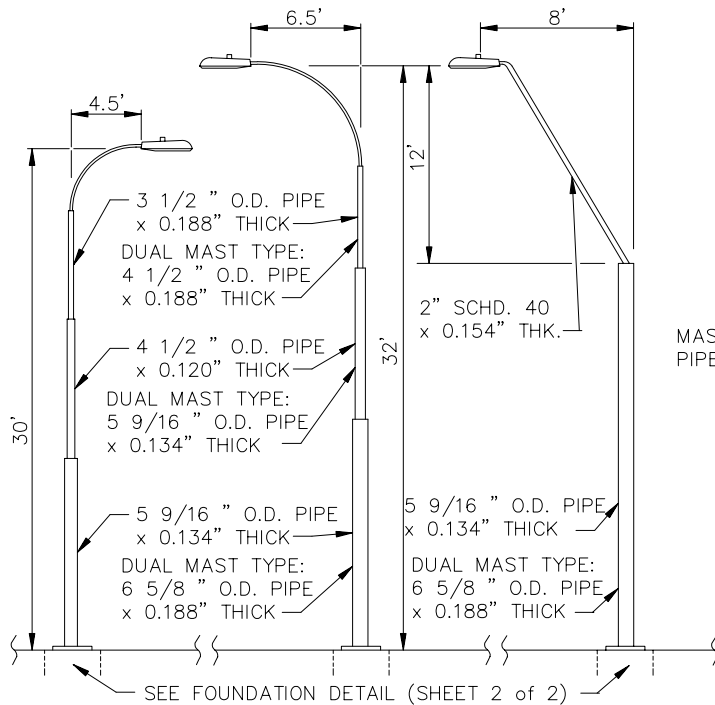
TYP SECTION C



TERMINAL BLOCK BEHIND PANEL



PROFILE



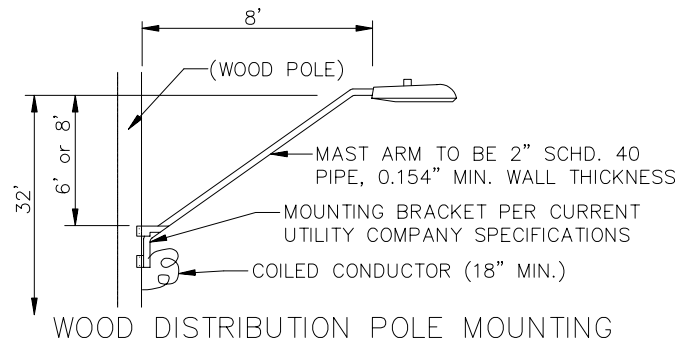
LUMINAIRE TYPE	A	B	C
M-250 A2 CUTOFF SERIES	6 1/2"	27 1/2"	12"
M-250 R2 CUTOFF SERIES	5 3/4"	26 1/4"	16"

SPECIFICATION FEATURES:

- 1) POWER/MODULE BALLAST ASSEMBLY FOR M-256 A2 ONLY.
- 2) 1 1/2" TO 2" FOUR- BOLT SLIPFITTER.
- 3) DIE-CAST ALUMINUM HOUSING WITH ELECTROCOAT GRAY PAINT FINISH.
- 4) ADJUSTABLE MOGUL BASE SOCKET.
- 5) ALGLAS FINISH ON REFLECTOR.
- 6) NO-TOOL PE RECEPTACLE.
- 7) PLUG-IN STARTING AID.
- 8) TRUE 90 DEGREE CUTOFF.
- 9) EXTERNAL STAINLESS STEEL BAIL LATCH.

MNTG. HGT.	ARM	5 9/16\" PIPE	4 1/2\" PIPE	3 1/2\" PIPE	POLE HEIGHT
28'	4'-6"	10'-0"	6'-9"	6'-9"	23'-6"
30'	4'-6"	10'-0"	7'-9"	7'-9"	25'-6"
32'	6'-6"	10'-0"	7'-9"	7'-9"	25'-6"

NOTE:
FOR PAINT SPECIFICATIONS SEE
STREET LIGHTING SPECIFICATION



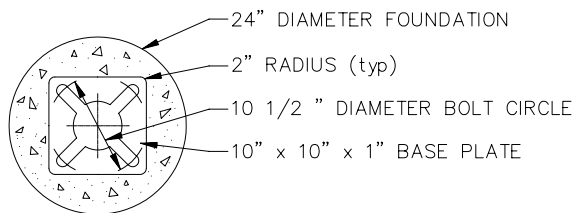
DETAIL NO.
G-3291-1

**CITY OF GOODYEAR
STANDARD DETAIL**

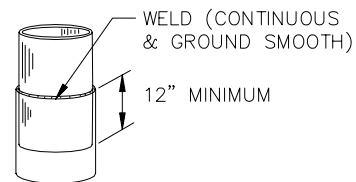
APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

STREAMLINE STEEL POLE DETAILS
(FOR USE IN AREAS DESIGNATED BY PUBLIC WORKS DIRECTOR.)

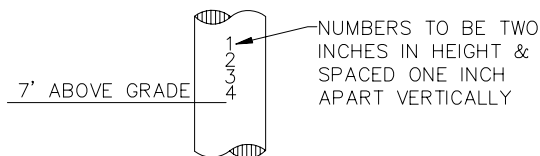
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G-3291-1



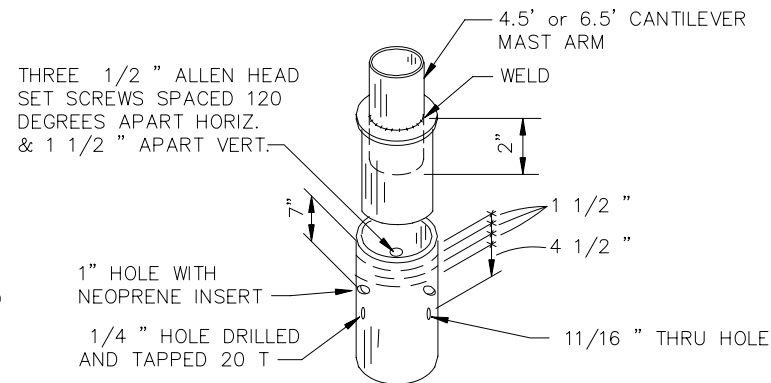
BASE PLATE SECTION



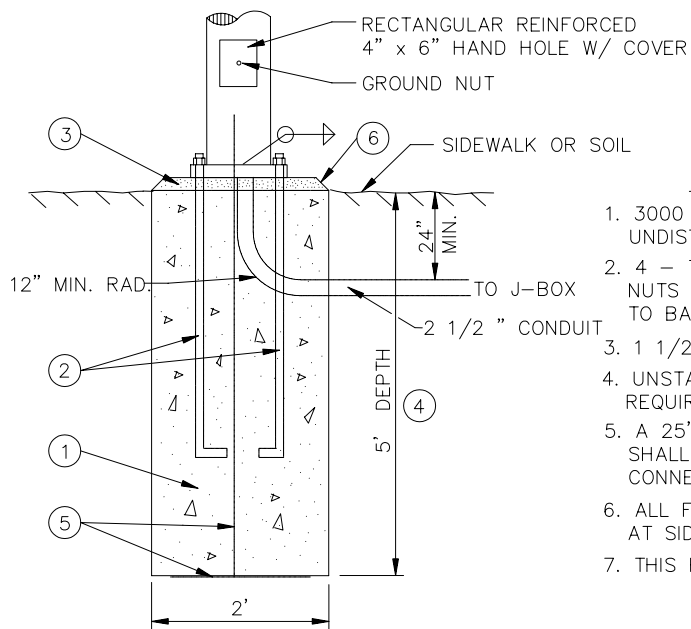
POLE JOINTS



POLE NUMBER LOCATIONS



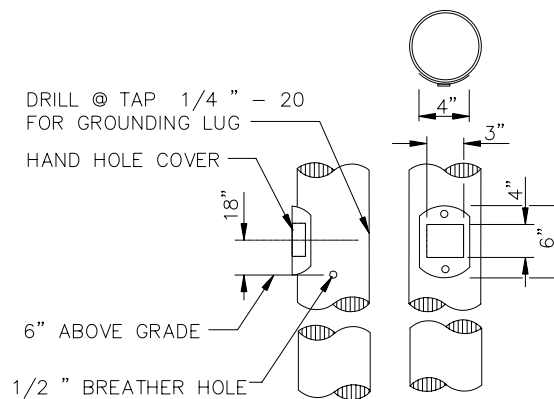
MAST ARM CONNECTION



FOUNDATION SECTION

FOUNDATION NOTES

1. 3000 P.S.I. CLASS A CONCRETE PLACED NEXT TO UNDISTURBED EARTH.
2. 4 - 7/8 " x 36" GALVANIZED ANCHOR BOLTS WITH LEVELING NUTS & WASHERS. TACK WELD NUTS TO WASHERS AND WASHERS TO BASE PLATE AFTER TIGHTENING. (2 1/2 " PROJECTION)
3. 1 1/2 " THICK EMBECO GROUT #636 OR APPROVED EQUAL.
4. UNSTABLE SOILS, AND/OR POLE HEIGHTS OVER 40', SHALL REQUIRE SPECIAL ENGINEERING.
5. A 25' COIL OF NO. 4 STRANDED A.W.G. BARE COPPER CONDUCTOR SHALL BE INSTALLED BEFORE THE CONCRETE IS POURED. IT SHALL BE CONNECTED TO POLE GROUNDING SCREW IN THE BASE OF THE POLE.
6. ALL FINISHED POLE FOUNDATIONS SHALL BE CHAMFERED, AND AT SIDEWALK GRADE UNLESS OTHERWISE NOTED.
7. THIS FOUNDATION MAY ALSO BE USED FOR DUAL MAST ARM POLES.



PENETRATIONS

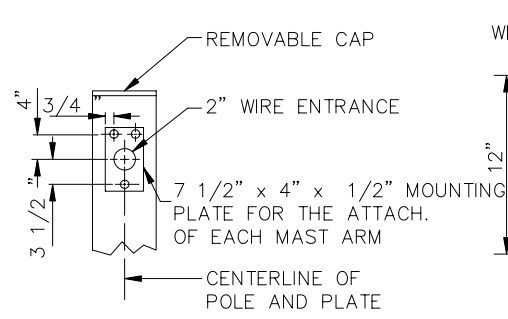
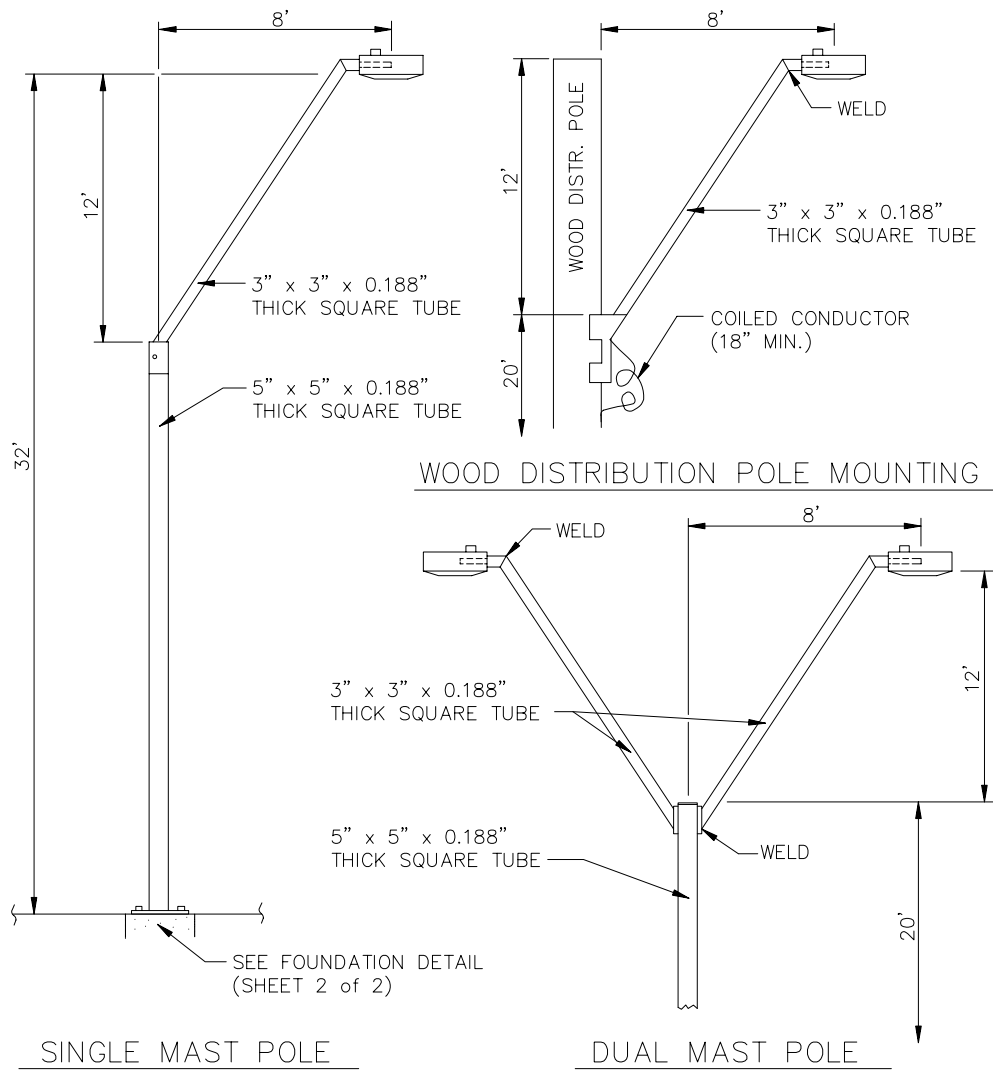
DETAIL NO.
G-3291-2

**CITY OF GOODYEAR
STANDARD DETAIL**

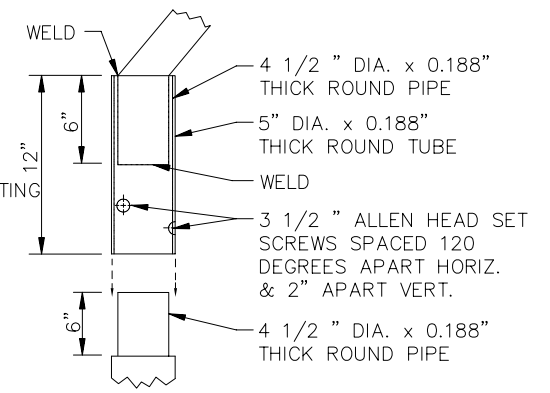
APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

STREAMLINE STEEL POLE DETAILS
(FOR USE IN AREAS DESIGNATED BY PUBLIC WORKS DIRECTOR.)

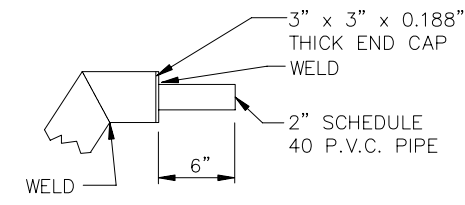
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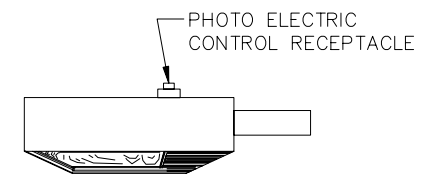
DUAL MAST ARM



SINGLE MAST ARM



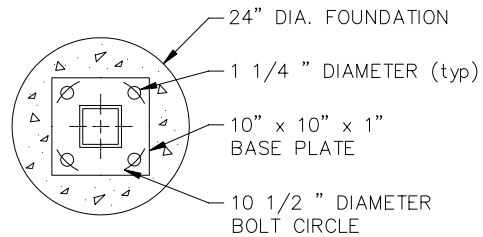
LUMINAIRE CONNECTION



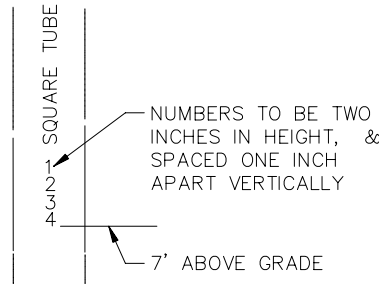
LUMINAIRE TYPE

SPECIFICATION FEATURES:

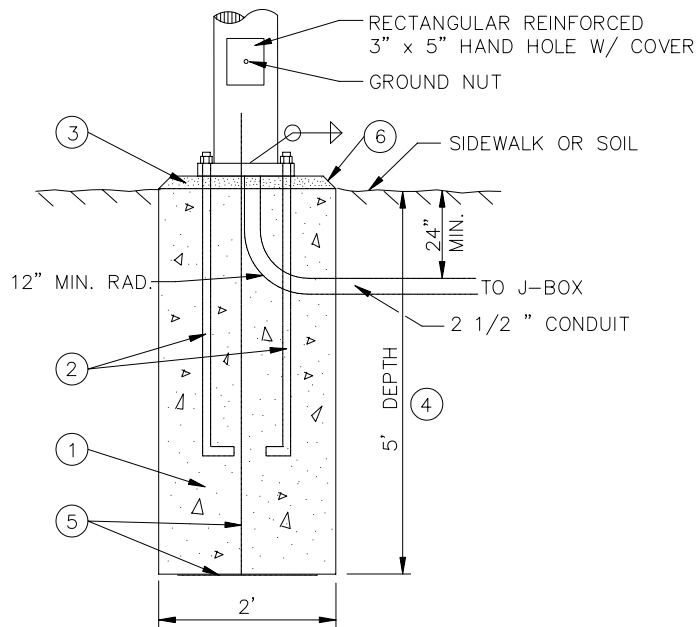
- 1) HEAVY GAUGE FORMED ALUMINUM HOUSING.
- 2) SEPARATE SWING DOWN OPTICAL AND BALLAST DOOR ACCESS.
- 3) ALGLAS FINISH ON REFLECTOR.
- 4) HEAT AND IMPACT RESISTANT TEMPERED FLAT GLASS LENS.
- 5) DECORATIVE MOUNTING ARM STANDARD.
- 6) MOGUI BASE SOCKET.
- 7) MULTITAP BALLAST-120/208/240/277.



BASE PLATE SECTION



POLE NUMBER LOCATIONS



FOUNDATION SECTION

GENERAL NOTES

1. POLES ARE TO BE DESIGNED PER A.A.S.H.T.O. - 80 SPECIFICATIONS.
2. ALL TUBING IS TO BE A.S.T.M. A500 GRADE B (46,000 P.S.I. MIN. YIELD).
3. ACCEPTED POLE MANUFACTURER:
CEM-TEC CORPORATION OR APPROVED EQUAL.
4. ACCEPTED LUMINAIRE MANUFACTURER:
AMERICAN ELECTRIC SERIES 153/154 OR G.E. DECASHIELD III, OR APPROVED EQUAL.
5. FOR PAINT SPECIFICATIONS SEE STREET LIGHTING SPECIFICATION.

FOUNDATION NOTES

1. 3000 P.S.I. CLASS A CONCRETE PLACED NEXT TO UNDISTURBED EARTH.
2. 4 - 7/8" x 36" GALVANIZED ANCHOR BOLTS WITH LEVELING NUTS & WASHERS. TACK WELD NUTS TO WASHERS AND WASHERS TO BASE PLATE AFTER TIGHTENING. (2 1/2" PROJECTION)
3. 1 1/2" THICK EMBECO GROUT #636 OR APPROVED EQUAL.
4. UNSTABLE SOILS, AND/OR POLE HEIGHTS OVER 40', SHALL REQUIRE SPECIAL ENGINEERING.
5. A 25' COIL OF NO. 4 STRANDED A.W.G. BARE COPPER CONDUCTOR SHALL BE INSTALLED BEFORE THE CONCRETE IS POURED. IT SHALL BE CONNECTED TO POLE GROUNDING SCREW IN THE BASE OF THE POLE.
6. ALL FINISHED POLE FOUNDATIONS SHALL BE CHAMFERED, AND AT SIDEWALK GRADE UNLESS OTHERWISE NOTED.
7. THIS FOUNDATION MAY ALSO BE USED FOR DUAL MAST ARM POLES.

DETAIL NO.
G-3292-2

CITY OF GOODYEAR
STANDARD DETAIL

APPROVED BY:
Goodyear Standards and
Policies Committee 7/97

ARCHITECTURAL STREET LIGHT

DETAIL NO.
G-3292-2